

*"No community should deliberately set out to plan for less than its ideal."*

– Dan Cary, Director of Planning, South Florida Water Management District

# The Hometown Plan

## South Miami, FL

### PROJECT DETAILS

Project Area: 378 acres  
Client: The City of South Miami  
South Miami Hometown, Inc.  
Year Adopted: 1993

For more information, visit [doverkohl.com](http://doverkohl.com).

### The 100% Model



Dorn Avenue, before



Dorn Avenue, after

To show the potential for a better South Miami, emphasis was placed on demonstration projects. Neighbors, homebuyers, businesses, investors, and bankers needed their confidence restored. The idea was to choose an area (however small) then totally transform it to create as dramatic and visible an impact as possible. Instead of spending precious funding across large areas, public funds were initially concentrated on these areas.

#### HOW TO IDENTIFY A "100% MODEL"

- It produces a "postcard" outdoor environment of exceptional character, exudes safety and optimism, and stands out in contrast to blight.
- It combines adaptive re-use of existing and historic structures with new buildings and improved public spaces.
- It completes the public space; i.e. both sides of the street, all corners of an intersection, or all sides of a green are built out or renovated.
- It includes a significant residential component, preferably owner-occupied.
- It lures residents of moderate incomes; these 'colonists' will demonstrate the viability of close-in neighborhoods and their discretionary income will help support local businesses.
- It suggests what the larger neighborhood will eventually look like, inspiring private investment.



Sunset Drive: before and after reconstruction. The Metrorail station (not pictured) is located at the entrance of this key mixed-use street.

### The Project

The Hometown Plan and the subsequent Hometown District Overlay Ordinance are examples of our work in traditional town centers. Starting in 1992, the study area became the subject of intensive public involvement to address the need for revitalizing the Main Street district along Sunset Drive in the heart of the City of South Miami. Dover, Kohl & Partners assisted the City and the not-for-profit organization, South Miami Hometown, Inc., with promoting redevelopment of the area, preparing a work program, conducting a charrette to design a detailed master plan, and rewriting the land development regulations. This work reintroduced traditional town planning techniques in South Miami. The Master Plan includes transit-oriented development, protection for historic structures, and pedestrian improvements to the streets and commercial buildings. Samuel Poole III, Esq., Peter M. Fernandez, PE, Clyde Judson, AIA, Walter Kulash, PE, John Zeeger, PE, and the FAU/FIU Joint Center for Environmental & Urban Problems also assisted in the creation of the Plan for South Miami.

### Process & Principles

Dover, Kohl & Partners hosted an open planning process in 1992 to engage the community in shaping the Plan. The refrain from participants heard again and again was 'we want our Main Street back.' Thus the primary goal was to recognize the City's streets as crucial assets and to treat (and reconstruct) them as people-friendly public spaces. Other guiding principles were to nurture the downtown as a neighborhood in the making rather than as a collection of buildings-on-parking; and to create a mix of uses for Downtown, including residential, so that the area could prosper as an "identifiable whole". This idea meant that a district-wide approach to parking would need to be addressed for all of Downtown. A diverse range of household incomes was also encouraged, which called for dignified forms of affordable housing. A final goal was that the two sides of the City, divided by US1, should be spatially and psychologically rejoined around the Metrorail transit station.

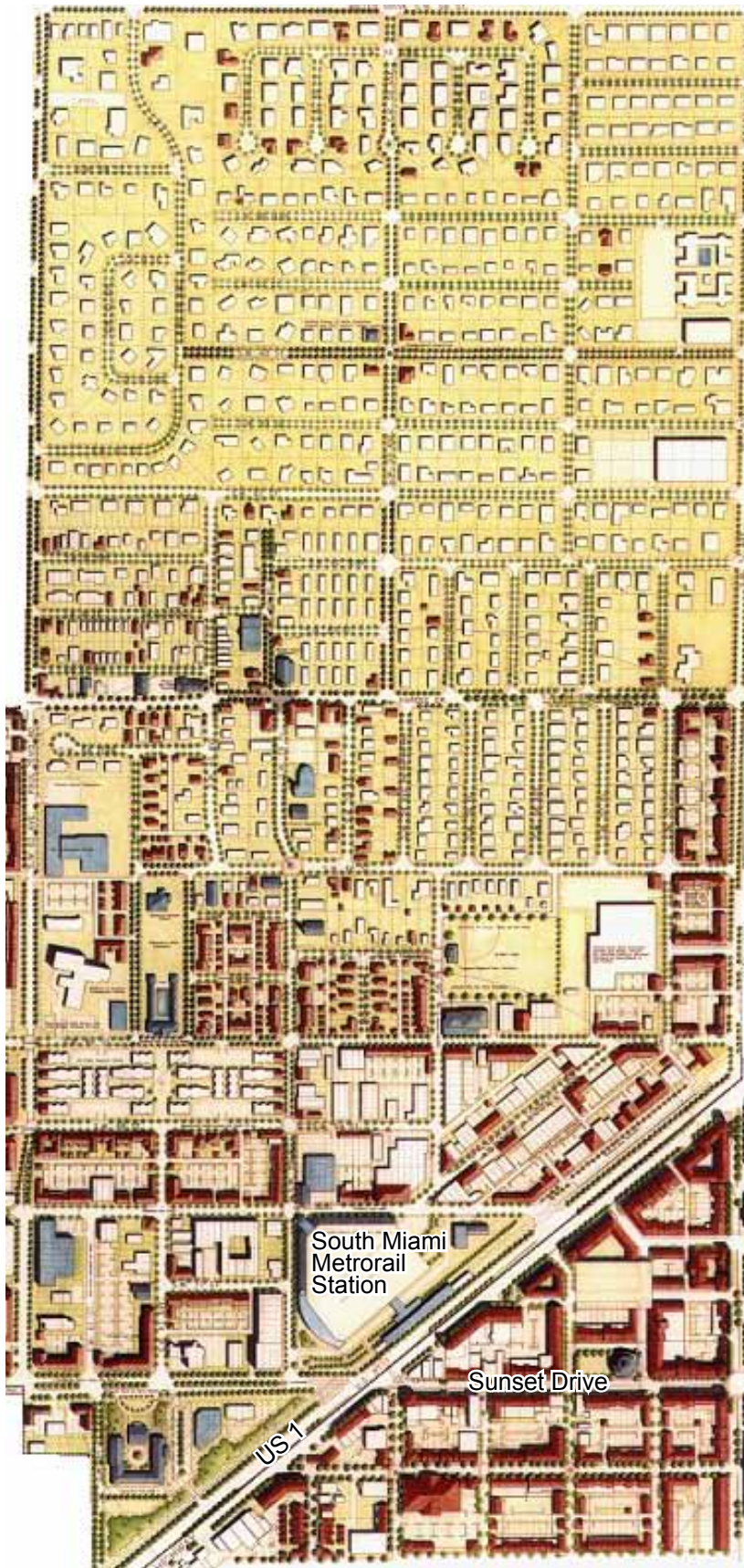
### Status

The City of South Miami has adopted and continues to implement the Hometown Plan and Hometown District Overlay Code. The initial "100% Model" project, the renovation of Southwest 59th Avenue, popularly known as "Dorn Avenue", has brought new life to one end of the downtown and precipitated numerous other building renovations and infill development throughout the district. Substantial streetscape improvements along Sunset Drive have been completed, incorporating wider sidewalks, reducing travel lanes, and introducing traffic calming features; all of which have resulted in greater pedestrian activity. As a complement to the newly revitalized commercial activity along the City's main street, the City formed a Community Redevelopment Agency (CRA) in 1998 to assist in revitalizing the area using the Hometown Plan as the underlying framework of the official CRA plan. In 2008 a new municipal garage lined with restaurants opened, enhancing the district-wide, park-once plan for the Main Street district.

DOVER, KOHL & PARTNERS  
town planning



# Transit-Oriented Development



The Amster Building is a new three-story, mixed-use infill project located directly across US-1 from the South Miami Metrorail station. Its design was made possible by the shared parking and transit proximity parking reduction provisions in the Hometown Plan District Overlay Code. The project was designed by the affiliated firms of Dover, Kohl & Partners and Chael, Cooper and Associates.



Hometown Station is a redevelopment of the South Miami Metrorail station. The "live-work" project entails wrapping the station's parking garage with four story mixed-use liner buildings, and capping the garage with a courtyard apartment building. The development forms a new public square, placing the station at the center of community life.

South Miami Metrorail Station      Streetscape improvements along Sunset Drive      Sunset Place development infill



Amster Building infill      Initial "100% Model" project, Dorn Avenue      New Municipal garage with ground-floor shops      Starbucks Building infill