

# Zoning Ordinance & Subdivision Regulations Rewrite

HENRICO COUNTY, VIRGINIA



## FBA-O District

Article 3, Division 8: FBA-O Form-Based Alternative Overlay District

*Revised Draft - Renumbering  
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**CLARION**

*In association with:*  
Mcbride|Dale|Clarion  
Dover, Kohl & Partners  
Greehan, Taves & Pandak



## DIVISION 8. FBA-O FORM-BASED ALTERNATIVE OVERLAY DISTRICT

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### **Sec. 24-3801.** *Purpose and Intent*

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- A. The purpose of the FBA-O Form-Based Alternative Overlay District is to provide an alternative set of standards for targeted areas within the County that may be used as an option by a landowner in place of the zoning district standards that apply in the base district, in order to achieve a balance between vehicular and pedestrian-oriented design and facilitate mixed-use, walkable development at higher densities. The FBA-O District standards allow landowners and developers to access otherwise untapped potential on a property by allowing additional forms of development by right in exchange for a better public realm.
- B. The FBA-O District is intended to foster predictable built results and a high-quality public realm by using the physical form as the organizing principle for development with a lesser focus on land use. District regulations address the relationship between building frontage and the public realm of the street and civic spaces, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. The overall vision for subdistricts in the FBA-O is illustrated with an illustrative plan and visualizations. The FBA-O is regulated by a regulating and street hierarchy plan and district and subdistrict standards that specify the required form of development in these areas.
- C. The FBA-O District is intended to provide:
1. A highly interconnected street network that disperses traffic and provides convenient routes for pedestrians and bicyclists;
  2. High-quality public spaces, with building façades having windows and doors facing tree-lined streets, plazas, squares, or neighborhood parks;
  3. Compact development, creating a walkable urban environment and conserving land and energy through reduced automobile dependence;
  4. Diversity, not homogeneity, with a variety of building types, street types, open spaces, and land uses serving people of all ages and multiple modes of mobility; and
  5. Resilient and sustainable neighborhoods, adaptable over time to improved public transit and to changing economic conditions.

### **Sec. 24-3802.** *Applicability*

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- A. An applicant proposing to develop lands in the FBA-O District may elect either to comply with all of the standards that apply in the base zoning district in which the lands are located, or to comply with the base district standards as modified by this section. Development approved in accordance with the standards in this section shall comply with all standards applicable in the underlying zoning district unless modified by the standards in this section.
- B. An applicant proposing to develop lands in the FBA-O District in accordance with this section shall submit an application for either a plan of development or an administrative site plan for the proposed development, as appropriate.

### **Sec. 24-3803.** *Subdistricts Established*

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The following FBA-O subdistricts are hereby established:

- A. The Brookfield Area Subdistrict (see Sec. 24-3860);
- B. The Parham/Broad Area Subdistrict (see Sec. 24-3866);
- C. The Williamsburg Road Area Subdistrict (see Sec. 24-3872); and
- D. The Virginia Center Commons Subdistrict (see Sec. 24-3879).

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**Sec. 24-3804.** *FBA-O District and Subdistrict Boundaries*

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The boundaries of the FBA-O District and of each of its subdistricts are identified on the Zoning Districts Map.

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**Sec. 24-3805.** *Organization of FBA-O District Standards*

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The FBA-O District standards are organized as follows:

- A. *Development Areas.* Each subdistrict is composed of development areas that are shown on the subdistrict’s regulating and street hierarchy plan. Sec. 24-3806 provides a general description of each of the development areas.
- B. *Streets and Sidewalks.* Sec. 24-3807 through Sec. 24-3819 establish standards for streets, alleys, sidewalks, street trees, and street lighting that apply in all subdistricts. Typical sections are included for primary, secondary, and tertiary streets that are identified on each subdistrict’s regulating and street hierarchy plan.
- C. *Civic Open Space.* Sec. 24-3820 through Sec. 24-3823 establish minimum standards for required minimum civic open space, provide descriptions of different types of civic open space, and identify what types of civic open space are allowed in each development area.
- D. *Frontage Standards.* Sec. 24-3824 through Sec. 24-3831 establish standards for the location of buildings and parking areas, building height, and frontage elements in each of the development areas.
- E. *Use Standards.* Sec. 24-3832 establishes what uses are allowed in each of the development areas.
- F. *Other General Standards.* These sections establish additional general standards within the FBA-O District:
  - 1. *Parking Standards* (Sec. 24-3833 through Sec. 24-3836);
  - 2. *Standards for New Buildings* (Sec. 24-3837 through Sec. 24-3843);
  - 3. *Specialty Buildings* (Sec. 24-3844 through Sec. 24-3847);
  - 4. *Signage* (Sec. 24-3848); and
  - 5. *Exterior Lighting* (Sec. 24-3849).
- G. *Subdistrict Standards.* These sections establish additional standards that apply to specific subdistricts within the FBA-O District and provide an illustrative plan for development in each subdistrict:
  - 1. *Brookfield Area Subdistrict Standards* (Sec. 24-3860 through Sec. 24-3865);
  - 2. *Parham/Broad Area Subdistrict Standards* (Sec. 24-3866 through Sec. 24-3871);
  - 3. *Williamsburg Road Area Subdistrict Standards* (Sec. 24-3872 through Sec. 24-3878); and



4. *Virginia Center Commons (VCC) Area Subdistrict Standards* (Sec. 24-3879 through Sec. 24-3886).

### **Sec. 24-3806.** *Development Areas*

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The regulating and street hierarchy plan for each FBA-O subdistrict (see Sec. 24-3859, FBA-O Subdistricts) establishes the location of development areas within the subdistrict. All parcels within a subdistrict are assigned to a development area and are subject to the specific standards for that development area, which govern the height and location of buildings and the location of parking (see Sec. 24-3824, Frontage Standards), civic open space (see Sec. 24-3820 through Sec. 24-3823), and allowable uses of land (see Sec. 24-3832, Use Standards). A brief description of each type of development area is set out below.

#### A. Mixed-use Corridor

Mixed-use Corridor development areas provide for a wide variety of retail, residential, and commercial uses intended to serve an area larger than a specific neighborhood in an urban setting. These areas are similar to Mixed-use Core areas, with a key difference being the build-to distance along a primary street. Mixed-use Corridor areas are designed to be used when properties abut a wide, major thoroughfare, such as Broad Street and East Parham Road. A mix of nonresidential and residential uses on individual lots is encouraged but not required. These areas permit the greatest intensity and mix of uses, and feature buildings located farther from the street but adjacent to the sidewalk, plentiful shade for pedestrians, along with parking lots and structured parking screened from the view of public spaces and streets.

#### B. Mixed-use Core

Mixed-use Core development areas provide for a wide variety of retail, residential, and commercial uses intended to serve an area larger than a specific neighborhood in an urban setting. A mix of residential and nonresidential uses on individual lots is encouraged but not required. These areas permit the greatest intensity and mix of uses, and feature buildings located adjacent to the sidewalk, plentiful shade for pedestrians, along with parking lots and structured parking screened from the view of public spaces and streets.

#### C. Walkable Corridor

Walkable Corridor development areas provide a mix of residential uses and nonresidential uses that serve the surrounding residential neighborhoods. These areas are similar to Walkable Center areas, with a key difference being the build-to distance along a primary street. Walkable Corridor areas are designed to be used when properties directly abut wide, high-speed thoroughfares, such as Williamsburg Road. These areas feature pedestrian-oriented urban form with buildings located farther from the street but close to the sidewalk, plentiful shade for pedestrians, and parking lots screened from view.

#### D. Walkable Center

Walkable Center development areas provide a mix of residential uses and nonresidential uses that serve the surrounding residential neighborhoods. These areas feature pedestrian-oriented urban form with buildings located close to the sidewalk, plentiful shade for pedestrians, and parking lots screened from view. When present, parking structures should be lined with habitable space facing public spaces or streets.

E. Neighborhood General

Neighborhood General development areas provide a mix of uses but are primarily residential, in a pedestrian-oriented urban form. They may have a wide range of building types. Setbacks and landscaping are variable. These areas provide a transition from the more commercial oriented Mixed-use Core and Walkable Center to Connected Edge.

F. Connected Edge

Connected Edge development areas are predominately residential but may have some neighborhood scale retail uses or home occupations. Buildings are primarily detached with a few attached building types in these areas. The built environment in these areas should retain key features of walkability such as front porches within conversation distance of the sidewalk.

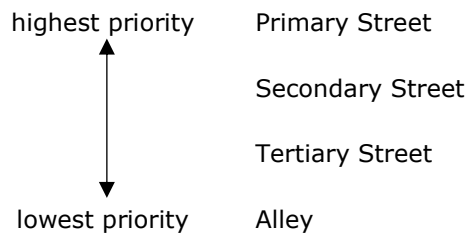
G. Highway Edge

Highway Edge development areas provide a transition of appropriate development between a walkable development area to an existing highway that creates a barrier. These areas may include a wide variety of retail, residential, and commercial uses. A mix of residential and nonresidential uses on individual lots is permitted but not required. While oriented primarily toward the highway, the built environment in these areas should retain key features of walkability.

**Sec. 24-3807. *Streets and Sidewalks: Street Hierarchy***

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- A. The Street Hierarchy for each Subdistrict identifies the streets that serve that Subdistrict. The precise alignment of new streets/thoroughfares may be adjusted in accordance with Sec. 24-3808, Streets and Sidewalks: New Street Connections.
- B. For each parcel that has multiple street frontages, the Street Hierarchy map determines the highest priority street frontage where the front build-to zone or setback shall apply (see Sec. 24-3824, Frontage Standards). Along the lower priority frontages, the side or rear build-to zones or setbacks shall apply. Primary Streets are the streets with the highest priority, followed by Secondary Streets, then Tertiary Streets, then Alleys, as illustrated below:



- C. If a parcel has more than one street frontage that is the highest priority street frontage (e.g., frontage on two primary streets, or no frontage on primary streets and frontage on two secondary streets), the applicant shall choose one of the highest priority frontages to be the primary street frontage where the front build-to zone or setback shall apply. To the extent a parcel within 400 feet of the initial parcel selecting the primary street in accordance

with this subsection is developed or redeveloped, it shall use the same street frontage as the initial parcel that was developed or redeveloped.

**Sec. 24-3808.** *Streets and Sidewalks: New Street Connections*

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- A. All potential new street connections identified on the regulating and street hierarchy plan for each subdistrict shall be provided when adjoining land is developed. The precise location and alignment of new street connections may be adjusted to allow flexibility in plan development, but the number of connections provided to surrounding parcels shall not be reduced.
- B. Any block face in the Mixed-Use Core, Walkable Center, Neighborhood General, and Connected Edge Districts which exceeds 600 feet shall have a mid-block pedestrian access that has a minimum width of eight feet.

**Sec. 24-3809.** *Streets and Sidewalks: General Street Standards*

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The design of new streets and modifications to existing streets shall comply with the following requirements:

- A. Improvements to thoroughfares shall be coordinated with the County Engineer.
- B. All thoroughfares shall connect to other streets.
- C. Culs-de-sac, and T-turnarounds are prohibited.
- D. Dead-end streets are only permitted for those shown on the regulating and street hierarchy plan when the adjacent property has not been developed or redeveloped, but shall be designed as temporary dead-end streets.
- E. All streets shall have at least two travel lanes, one in each direction; however, streets around Civic Spaces may have one travel lane with one-way traffic on each side of the Civic Space.
- F. On-street parking lanes shall not be closer than 20 feet to intersections measured from the intersecting property lines.
- G. All new thoroughfares shall have sidewalks on both sides of the travel lanes.
- H. All sidewalks shall have a minimum width of six feet, and a continuous unobstructed area within the sidewalk of a width no less than five feet. The unobstructed area shall be unobstructed by utility poles, fire hydrants, street furnishings, or any other temporary or permanent structures.
- I. Free and clear public use of the sidewalk area outside of the right-of-way shall be protected by a public access easement.
- J. With the exception of fire hydrants, utilities shall run underground and above-ground projections of utilities shall be placed in or along rights-of-way of streets of lower street hierarchy, wherever practicable.

**Sec. 24-3810.** *Streets and Sidewalks: Alleys*

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- A. Where practical, alleys shall be used for access to parking and services at the rear of lots.

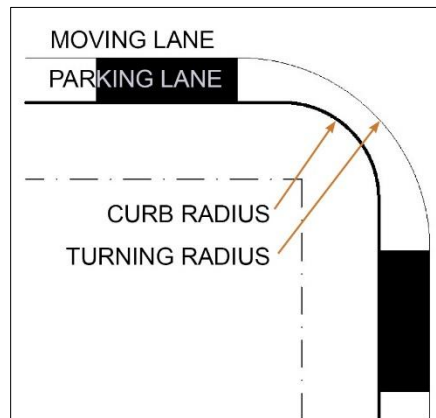
- B. Alleys shall comply with the required alley dimensions in Table 3810: Required Alley Dimensions.

<b>Table 3810: Required Alley Dimensions</b>		
<b>Uses Served by Alley</b>	<b>Right-of-Way Width (Max.)</b>	<b>Pavement Width</b>
Residential	24'	12' - 18'
Nonresidential or Mixed-Use		18' - 20'

- C. When sidewalks cross alleys, the sidewalk shall remain at the same elevation for pedestrians, but the pavement texture should change to signify the alley.

**Sec. 24-3811. Streets and Sidewalks: Curb Radius**

- A. Streets with on-street parking shall have a maximum corner curb radius of 15 feet, in order to enhance safety for pedestrians by inhibiting drivers from turning corners at high speeds. (The effective turning radius is larger than the curb radius when parking is present. See Figure 3811: Corner Curb with On-street Parking)
- B. Streets without on-street parking shall comply with the curb radius requirements in the Henrico County Public Works Design Manual.



*Figure 3811: Corner Curb with On-street Parking*

**Sec. 24-3812. Streets and Sidewalks: Streetscape**

Sidewalks have three zones with differing functions. They are the Frontage Zone, Clear Path, and Furnishing and Landscape Strip (see Figure 3812: Sidewalk Zones). The purpose of each zone and applicable standards are set forth below.



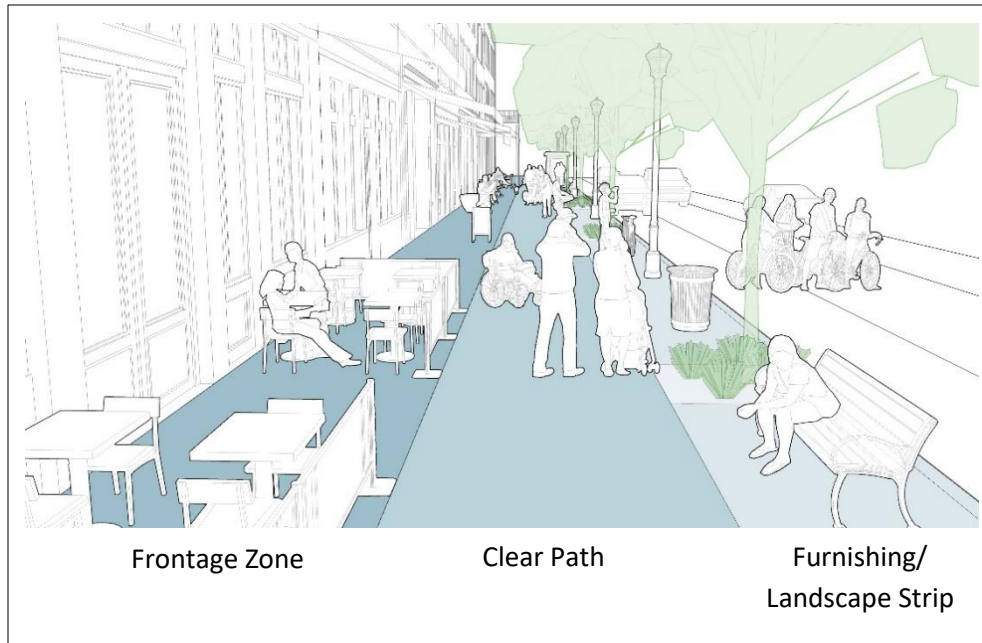


Figure 3812: Sidewalk Zones

#### A. Frontage Zone

The frontage zone is the space between the building façade or property line and the clear path. This space supplements the buildings' activities and provides a buffer between pedestrians, frontage elements that extend beyond the façade such as arcades or colonnades, and opening doors. It is the location for seating, signs, retail displays, and landscaping.

#### B. Clear Path

The clear path is the portion of the sidewalk dedicated to pedestrian travel. It should be accessible and free of physical obstructions to allow for the movement of people. It should be well-lit and functional in all weather conditions, including snowy winters, and shall comply with the following standards:

1. A paved sidewalk at least six feet wide shall be provided in alignment with existing sidewalks within the right-of-way. If there are no existing sidewalks within the right-of-way, or if sidewalks do not fit within the right-of-way, an easement at least six feet wide shall be provided.
2. The sidewalk shall be paved to the same elevation as the adjoining sidewalk or land.

#### C. Furnishing/Landscape Strip

The furnishing and landscape strip serves many functions, varying greatly depending on the type of street. Its primary purpose is to separate the clear path from motorists and provide a location for street furniture and utilities, street trees, storm water elements, lighting, transit stops, bike racks, parking meters, signage, and other elements.

**Sec. 24-3813.** *Streets and Sidewalks: Street Trees*

Either a sidewalk planting strip at least five feet wide between the sidewalk and the street or minimum 5-foot wide by 6-foot long street tree planting areas, shall be provided for the location of street trees. Street trees shall be planted at a maximum average spacing of 30 feet on center. Street tree areas shall be coordinated with public utilities.

**Sec. 24-3814.** *Streets and Sidewalks: Street Lighting*

- A. The property owner shall install pedestrian-scaled fixtures meeting the County’s technical construction standards on all streets fronting the property (see Figure 3814: Types of Street Lighting).

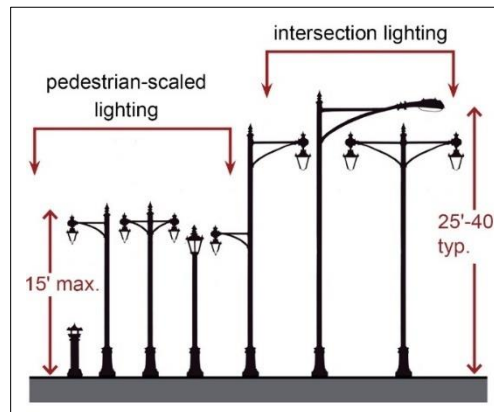


Figure 3814: Types of Street Lighting

- B. Intersection-scaled lighting shall be used in addition to pedestrian-scaled lights where required by the County (see Figure 3814: Types of Street Lighting).
- C. Street lights shall be aligned with street tree placement (generally between two and a half feet and four feet from the back of the curb). Placement of fixtures shall be coordinated with the organization of sidewalks, landscaping, street trees, building entries, curb cuts, signage, etc.
- D. Pedestrian scaled light fixtures shall not exceed 15 feet in height to promote a pedestrian scale to the public realm and to minimize light spill to adjoining properties. Light fixtures shall be spaced 30 feet on center on average.
- E. Light poles may include armature that allows for the hanging of banners or other amenities (e.g., hanging flower baskets, artwork, etc.).
- F. Lighting shall be designed to prevent the direct view of the light source from neighboring residential areas.
- G. Light fixtures shall be downcast or low cut-off fixtures to prevent glare and light pollution.
- H. To conserve energy and reduce long-term costs, energy-efficient lamps shall be used for all public realm lighting.
- I. There shall be consistency of lighting fixtures along each street to create a unifying scheme of illumination that is appropriate to the scale of the street and the level of nighttime activity. Lamp styles shall be consistent along both sides of each block of each street.

**Sec. 24-3815. Streets and Sidewalks: Terminated Vista**

To enhance wayfinding and mark an important view, where a point of reference is indicated on a regulating and street hierarchy plan, a tower, cupola, statue, monument, or a similar significant architectural feature shall be placed in the area where the point of reference is indicated.

**Sec. 24-3816. Streets and Sidewalks: Typical Street Types**

The following typical street sections may be used to create new streets or modify existing streets within the FBA-O District:

- A. Typical Primary Street Sections;
- B. Typical Secondary Street Sections; and
- C. Typical Tertiary Street Sections.

**Sec. 24-3817. Streets and Sidewalks: Typical Primary Street Sections**

A. Multiway Boulevard

The Multiway Boulevard is designed for high traffic counts while providing an access lane for local traffic to access shops and restaurants. Street trees provide shade for pedestrians and bicyclists. Wide sidewalks accommodate outdoor dining. The Multiway Boulevard will ideally be utilized for West Broad Street through the Brookfield Area Subdistrict and the Parham/Broad Area Subdistrict. (See Figure 3817A: Multiway Boulevard Section.)

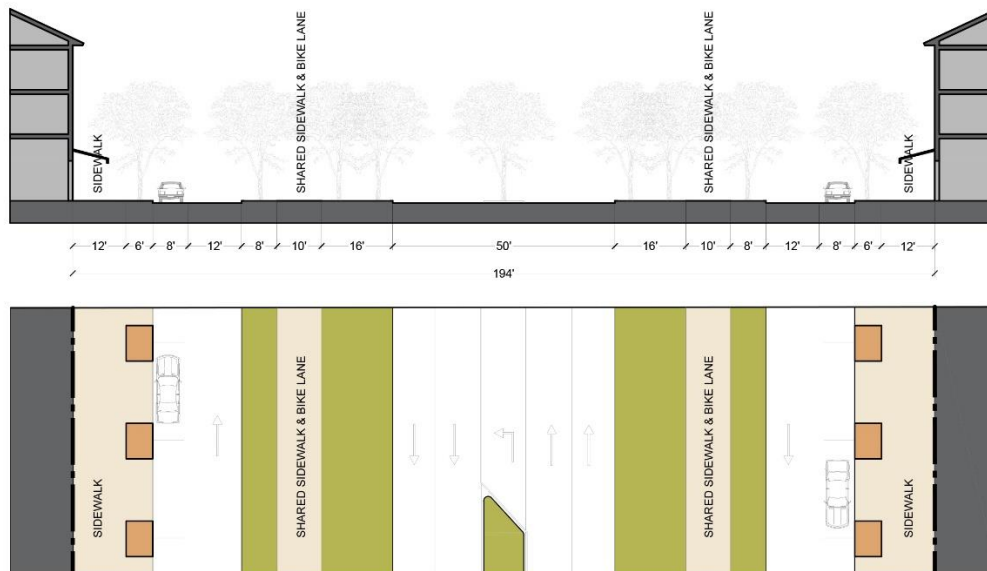


Figure 3817A: Multiway Boulevard Section

B. Primary Street 1

Primary Street 1 accommodates two-way traffic with parallel parking on either side. Pairs of trees are planted on each side of two bike lanes. These bike lanes are located between the parallel parking and sidewalk along the street. (See Figure 3817B: Primary Street 1 Section.)

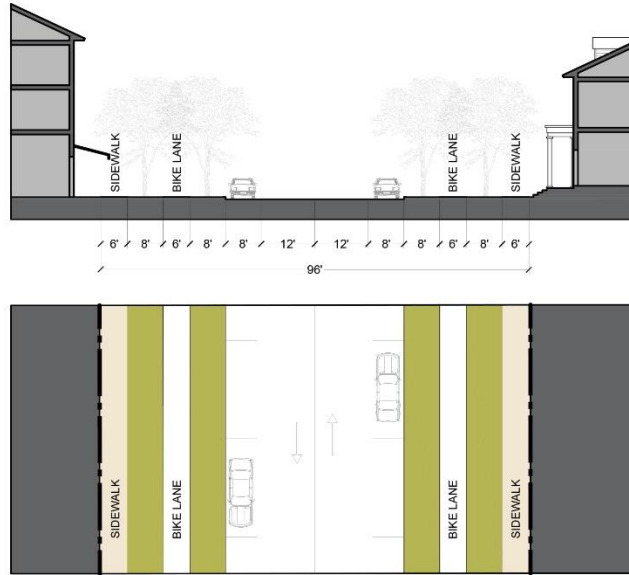


Figure 3817B: Primary Street 1 Section

C. Primary Street 2

Primary Street 2 accommodates two-way traffic with parallel parking on either side. Bike lanes are located between the parallel parking and sidewalk along the street. A raised curb is located between the bike lanes and parallel parking while street trees are located between the bike lanes and sidewalks. (See Figure 3817C: Primary Street 2 Section.)

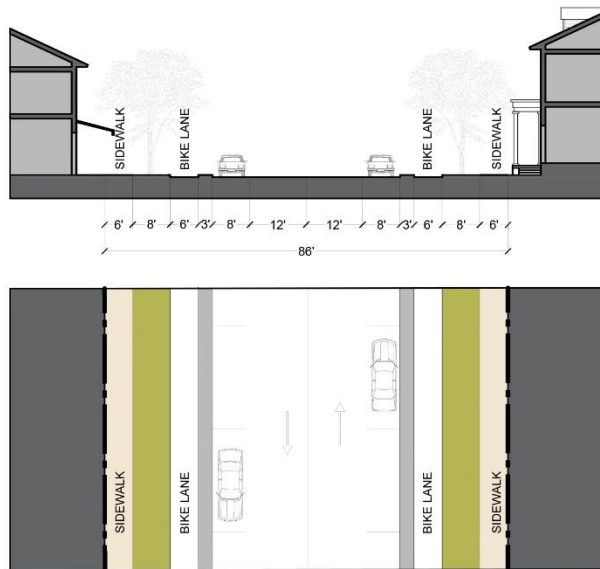


Figure 3817C: Primary Street 2 Section



### D. Primary Street 3

Primary Street 3 accommodates two-way traffic with parallel parking on either side. A two-way cycle track is located on one side of the street with street trees planted on either side. (See Figure 3817D: Primary Street 3 Section.)

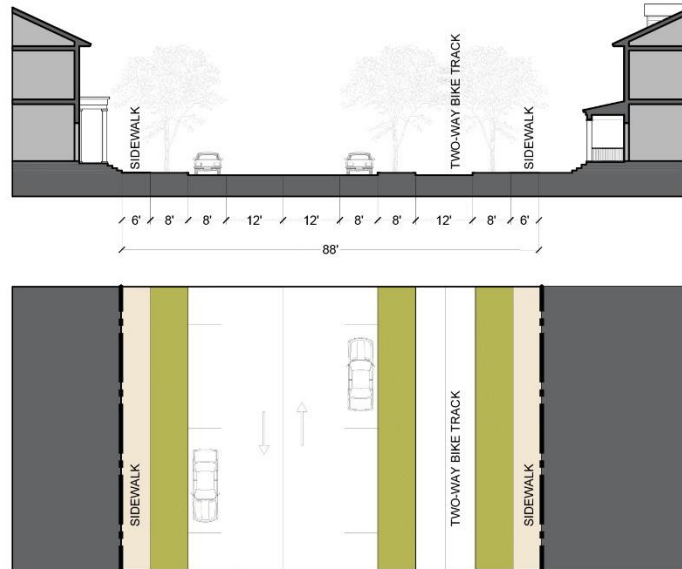


Figure 3817D: Primary Street 3 Section

### E. Primary Street 4

Primary Street 4 accommodates two-way traffic with parallel parking on either side. Street trees are located between parallel parking and the sidewalk. An optional turn lane that does not decrease the planting area and sidewalks may be added. (See Figure 3817E: Primary Street 4 Section.)

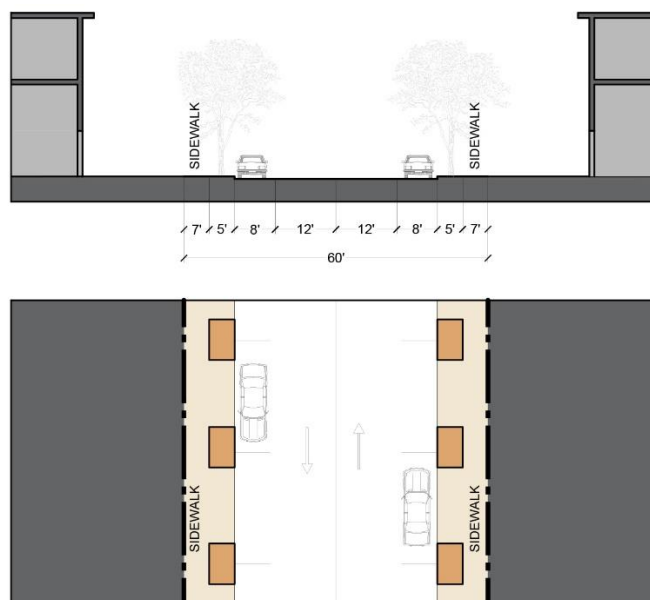


Figure 3817E: Primary Street 4 Section

**Sec. 24-3818.** *Streets and Sidewalks: Typical Secondary Street Sections*

A. Secondary Street 1

Secondary Street 1 accommodates two-way traffic with parallel parking on either side. Street trees are located between parallel parking and the sidewalk. An optional turn lane that does not decrease the planting area and sidewalks may be added. (See Figure 3818A: Secondary Street 1 Section.)

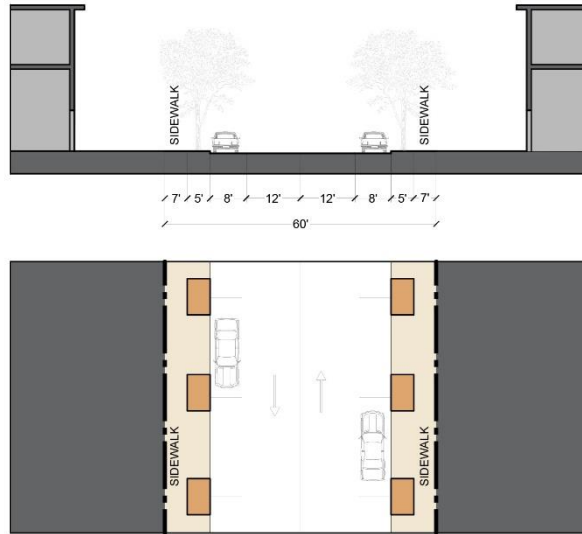


Figure 3818A: Secondary Street 1 Section

B. Secondary Street 2

Secondary Street 2 is for tighter right-of-way residential areas. It accommodates two-way traffic with parallel parking on one side. Street trees provide shade and are located between the street and the sidewalk. (See Figure 3818B: Secondary Street 2 Section.)

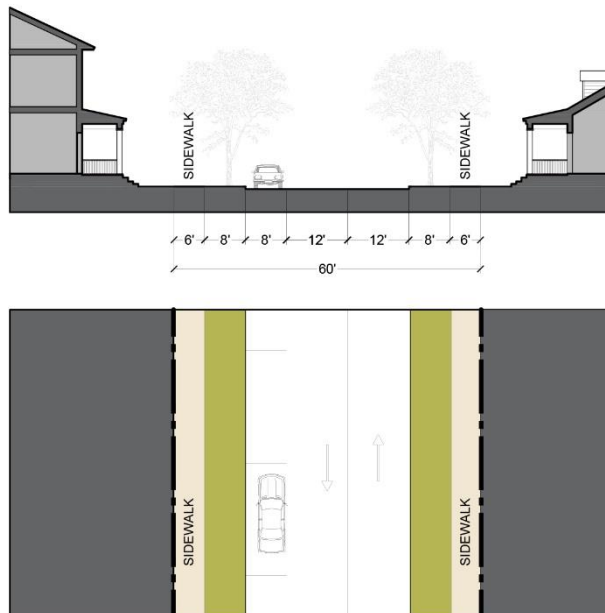


Figure 3818B: Secondary Street 2 Section

**Sec. 24-3819.** *Streets and Sidewalks: Typical Tertiary Street Sections*

A. Tertiary Street 1

Tertiary Street 1 is intended for slower residential streets that are relatively urban. It accommodates two-way traffic with parallel parking on either side. Street trees are located between the street and sidewalks. (See Figure 3819A: Tertiary Street 1 Section.)

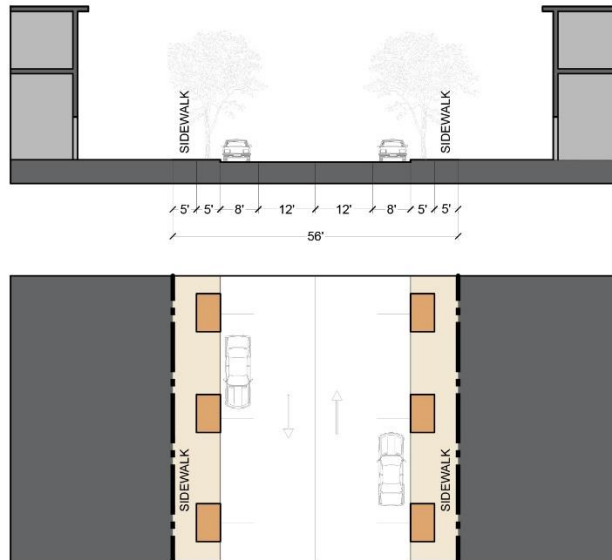


Figure 3819A: Tertiary Street 1 Section

B. Tertiary Street 2

Tertiary Street 2 is primarily for residential streets with slow traffic. It accommodates two-way traffic with parallel parking on one side. Street trees are located between the street and the sidewalk. (See Figure 3819B: Tertiary Street 2 Section.)

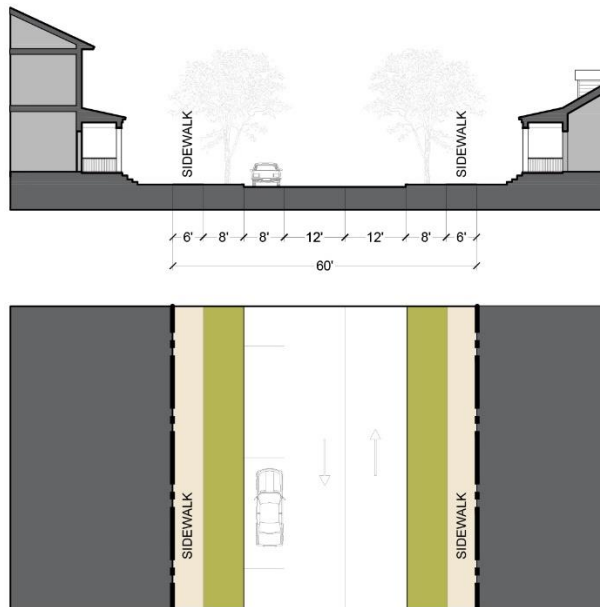



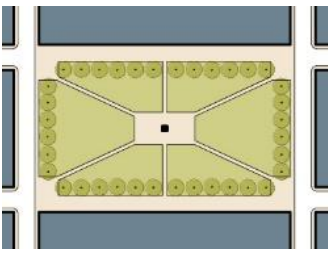
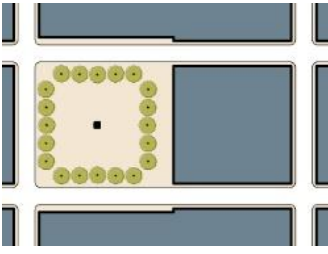
Figure 3819B: Tertiary Street 2 Section

**Sec. 24-3820. Civic Open Space: Minimum Civic Open Space Required**

- A. All new development shall provide civic space within one-quarter-mile walking distance of every parcel. The civic open space shall be privately owned.
- B. Except as otherwise provided in subsection D below, the following minimum percentages of the development site area shall be set aside as civic open spaces:
  - 1. 7.5% for residential uses.
  - 2. 5% for non-residential uses and mixed uses.
- C. The requirement in subsection B above, is in addition to any planting strips within street rights-of-way and open space provided on lots with private buildings. The civic open space shall be privately owned.
- D. The requirement in subsection B above, may be met by a comparable amount of civic open space within a one-quarter mile walking distance that already exists or is committed to be provided by another development to comply with subsection B above, but not used by that development.

**Sec. 24-3821. Civic Open Space: Types of Civic Open Space**

Civic open spaces shall be one of the types described in Table 3821: Types of Civic Open Space.

<b>Table 3821: Types of Civic Open Spaces</b>	
	<p><b>Green</b></p> <p>A green is an informal space consisting of lawn and informally arranged trees and shrubs, typically furnished with paths, benches, and open shelters. Greens are spatially defined by abutting streets.</p>
	<p><b>Square</b></p> <p>A square is a formal open space available for recreational and civic uses and spatially defined by abutting streets and building frontages. Landscaping in a square consists of lawn, trees, and shrubs planted in formal patterns and it is typically furnished with paths, benches, and open shelters.</p>
	<p><b>Plaza</b></p> <p>A plaza is a formal open space available for civic and commercial uses and spatially defined by building frontages. Landscaping in a plaza consists primarily of pavement; trees and shrubs are optional.</p>



**Table 3821: Types of Civic Open Spaces**

	<p><b>Neighborhood Park</b></p> <p>A neighborhood park is a natural landscape consisting of open and wooded areas, typically furnished with paths, benches, and open shelters. Neighborhood parks are often irregularly shaped but may be linear in order to parallel water features or corridors.</p>
	<p><b>Playground</b></p> <p>A playground is a fenced open space, typically interspersed within residential areas, that is designed and equipped for the recreation of children. Playgrounds may be freestanding or located within parks or greens.</p> <p><b>Community Garden</b></p> <p>A community garden is a grouping of garden plots available to nearby residents for small-scale cultivation.</p>

**Sec. 24-3822.** *Civic Open Space: Civic Open Space Types Allowed in Development Areas*

Table 3822 identifies which types of open space types are allowed in each development area.

<b>Table 3822: Civic Open Space Types Allowed in Development Areas</b>									
Civic Space Type	Must front at least:	Typical Size	Mixed-use Corridor	Mixed-use Core	Walkable Corridor	Walkable Center	Neighborhood General	Connected Edge	Highway Edge
<b>Green</b>	2 Streets	0.5 to 5 acres			X	X	X	X	
<b>Square</b>	2 Streets	0.5 to 2 acres	X	X	X	X	X		X
<b>Plaza</b>	1 Street	0.1 to 2 acres	X	X					X
<b>Neighborhood Park</b>	1 Street	Min. 0.5 acres					X	X	
<b>Playground</b>	0 Streets	0.1 to 1 acre	X	X	X	X	X	X	X
<b>Community Garden</b>	0 Streets	0.1 to 1 acre	X	X	X	X	X	X	X

**NOTES:**  
 An "X" in the development area column indicates that the civic space type is allowed in that development area.

**Sec. 24-3823.** *Civic Open Space: Design Standards*

- A. Squares and plazas shall be located so that at least 25 percent of buildings on the public space are facing it with their primary facades.

- B. Civic Open Spaces must be designed, landscaped, and furnished and not treated as leftover space. They shall be included in each plan phase.
- C. Stormwater Management improvements shall be integrated with the final landscape design as site amenities (see Article 5. Division 3, Required Open Space).

**Sec. 24-3824. Frontage Standards**

The frontage standards in this section establish the physical and functional relationships between buildings and the street. The standards include rules related to building placement (build-to zones, setbacks, location of parking, etc.) and building heights. New development in the FBA-O must comply with the frontage standards in this Division based on the development area designation on the regulating and street hierarchy plan where the development is located. Table 3824: Summary of Frontage Standards, summarizes the frontage standards that apply in each development area. New development in each development area in the FBA-O District shall comply with the corresponding frontage standards in Sec. 24-3825 through Sec. 24-3831.

**Table 3824: Summary of Frontage Standards**

Frontage Standards	Development Area						
	Mixed-use Corridor	Mixed-use Core	Walkable Corridor	Walkable Center	Neighborhood General	Connected Edge	Highway Edge
<b>Heights <sup>1</sup></b>							
Primary Street <sup>1</sup>	75' max <sup>1,2</sup>	75' max <sup>1,2</sup>	60' max <sup>1,2</sup>	60' max <sup>1,2</sup>	55' max <sup>1,2</sup>	40' max <sup>1,2</sup>	165' max <sup>1,2</sup>
Secondary Street <sup>1</sup>	65' max <sup>1,2</sup>	65' max <sup>1,2</sup>	60' max <sup>1,2</sup>	60' max <sup>1,2</sup>	55' max <sup>1,2</sup>	35' max <sup>1,2</sup>	165' max <sup>1,2</sup>
Tertiary Street <sup>1</sup>	60' max <sup>1,2</sup>	60' max <sup>1,2</sup>	55' max <sup>1,2</sup>	55' max <sup>1,2</sup>	50' max <sup>1,2</sup>	35' max <sup>1,2</sup>	110' max <sup>1,2</sup>
First Floor Height <sup>3</sup>	13' min	13' min	13' min	13' min	13' min	13' min	13' min
Ground Finished Floor <sup>4</sup>	Com: 0' min Res: 2' min	Com: 0' min Res: 2' min	Com: 0' min Res: 2' min	Com: 0' min Res: 2' min	Com: 0' min Res: 2' min	Res: 2' min	Com: 0' min Res: 2' min
<b>Building Placement</b>							
Primary Street Build-to Zone <sup>5</sup>	15' to 25' <sup>5</sup>	0' to 10' <sup>5</sup>	15' to 25' <sup>5</sup>	0' to 10' <sup>5</sup>	0' to 12'	10' min setback	0' to 8' <sup>5</sup>
Secondary Street Build-to Zone <sup>5</sup>	0' to 12' <sup>5</sup>	0' to 10' <sup>5</sup>	0' to 15' <sup>5</sup>	0' to 12' <sup>5</sup>	0' to 18'	10' min setback	0' to 10' <sup>5</sup>
Interior Side Yard Setback	0' min	0' min	0' min	0' min	0' min	5' min	0' min
Tertiary Street Build-to Zone	0' to 10'	0' to 10'	6' to 18'	6' to 18'	6' to 18'	12' min setback	0' to 10'
Alley or Rear Setback	5' min	5' min	5' min	5' min	5' min	5' min	5' min
Lot Coverage	90% max	90% max	75% max	75% max	70% max	60% max	90% max
<b>Frontage Build-Out</b>							
Building Front	80% min	80% min	60% min	60% min	60% min	40% min	80% min
Building Side (street)	40% min	40% min	30% min	30% min	30% min	20%	40% min
Building Rear (street)	60% min	60% min	40% min	40% min	40% min	N/A	60% min
<b>Parking Location</b>							
Front Setback <sup>4</sup>	30' min <sup>6</sup>	30' min <sup>6</sup>	30' min <sup>6</sup>	30' min <sup>6</sup>	20' min <sup>6</sup>	24' min <sup>6</sup>	30' min <sup>6</sup>
Interior Side Yard Setback <sup>4</sup>	0' min <sup>6</sup>	0' min <sup>6</sup>	0' min <sup>6</sup>	0' min <sup>6</sup>	0' min <sup>6</sup>	0' min <sup>6</sup>	0' min <sup>6</sup>

**Table 3824: Summary of Frontage Standards**

Frontage Standards	Development Area						
	Mixed-use Corridor	Mixed-use Core	Walkable Corridor	Walkable Center	Neighborhood General	Connected Edge	Highway Edge
Street Side Yard Setback <sup>4</sup>	30' min <sup>6</sup>	30' min <sup>6</sup>	30' min <sup>6</sup>	30' min <sup>6</sup>	20' min <sup>6</sup>	20' min <sup>6</sup>	30' min <sup>6</sup>
Rear Setback (lot) <sup>4</sup>	5' min <sup>6</sup>	5' min <sup>6</sup>	5' min <sup>6</sup>	5' min <sup>6</sup>	5' min <sup>6</sup>	5' min <sup>6</sup>	5' min <sup>6</sup>
Rear Setback (alley) <sup>4</sup>	0' min <sup>6</sup>	0' min <sup>6</sup>	0' min <sup>6</sup>	0' min <sup>6</sup>	3' min <sup>6</sup>	3' min <sup>6</sup>	0' min <sup>6</sup>
Rear Setback (street) <sup>4</sup>	30' min <sup>6</sup>	30' min <sup>6</sup>	20' min <sup>6</sup>	20' min <sup>6</sup>	20' min <sup>6</sup>	20' min <sup>6</sup>	20' min <sup>6</sup>
Frontage Elements							
Allowed Frontage Elements <sup>7</sup>	Forecourt, Stoop, Shopfront, Gallery, Arcade / Colonnade <sup>5</sup>	Forecourt, Stoop, Shopfront, Gallery, Arcade / Colonnade <sup>5</sup>	Terrace or Lightwell, Forecourt, Stoop, Shopfront, Gallery, Arcade / Colonnade <sup>5</sup>	Terrace or Lightwell, Forecourt, Stoop, Shopfront, Gallery, Arcade / Colonnade <sup>5</sup>	Porch, Terrace or Lightwell, Forecourt, Stoop, Shopfront <sup>5</sup>	Porch, Stoop, Common Yard <sup>5</sup>	Terrace or Lightwell, Forecourt, Stoop, Shopfront, Gallery, Arcade / Colonnade <sup>5</sup>

**NOTES:**

1. Allowable building height determined based on street designation where front of building is located.
2. Towers, cupolas, and other architectural rooftop features with a footprint smaller than 15' by 15' may extend up to 20' above the designated height limit.
3. Measured floor to floor.
4. Measured above grade.
5. A minimum front 13' sidewalk shall be provided. The sidewalk may be a combination of public and private property.

6. Reference Parking section for requirements.

7. See General Standards for requirements of frontage elements.

General Table NOTES:

- All buildings must have a Primary Pedestrian Entrance along the front facade.
- Loading docks and other service entries shall not be located on primary frontages.
- "Com" is an abbreviation for "Commercial."
- "Res" is an abbreviation for "Residential."

**Sec. 24-3825. Mixed-use Corridor Frontage Standards**

Building Heights		
<b>A</b>	Primary Street Building Height <sup>1</sup>	75' max <sup>1,2</sup>
<b>A</b>	Secondary Street Building Height <sup>1</sup>	65' max <sup>1,2</sup>
<b>A</b>	Tertiary Street Building Height <sup>1</sup>	60' max <sup>1,2</sup>
<b>B</b>	First Floor Height (floor to floor)	13' minimum
<b>C</b>	Ground Finished Floor above sidewalk or finished grade	Commercial: 0' min Residential: 2' min
Building Placement		
<b>D</b>	Primary Street Build-to Zone 3	15' minimum to 25' maximum <sup>3</sup>
<b>E</b>	Secondary Street Build-to Zone 3	0' minimum to 12' maximum <sup>3</sup>
<b>F</b>	Interior Side Yard Setback	0' minimum
	Tertiary Street Build-to Zone	0' minimum to 10' maximum
<b>G</b>	Alley or Rear Setback	5' minimum
	Lot Coverage	90% maximum
Frontage Built-Out		
<b>H</b>	Building Front	80% minimum
	Building Side (street)	40% minimum
	Building Rear (street)	60% minimum
Parking Location		
<b>I</b>	Front Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>J</b>	Interior Side Yard Setback <sup>4</sup>	0' minimum <sup>4</sup>
<b>K</b>	Street Side Yard Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>L</b>	Rear Setback (lot) <sup>4</sup>	5' minimum <sup>4</sup>
	Rear Setback (alley) <sup>4</sup>	0' minimum <sup>4</sup>
	Rear Setback (Street) <sup>4</sup>	30' minimum <sup>4</sup>
Frontage Elements		
	Allowed Frontage Elements <sup>5</sup>	Forecourt, Stoop, Shopfront, Gallery, Arcade / Colonnade <sup>5</sup>

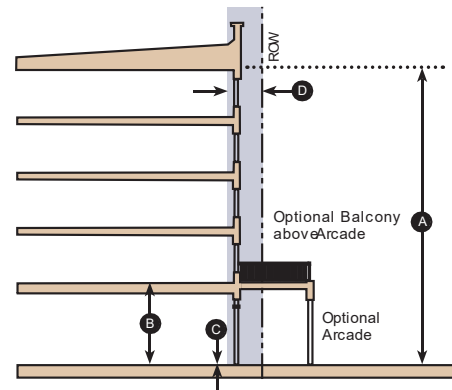
**NOTES:**

- Allowable building height determined based on street designation where front of building is located.
- Towers, cupolas, and other architectural rooftop features with a footprint smaller than 15' by 15' may extend up to 20' above the designated height limit.
- A minimum front 13' sidewalk shall be provided in the Mixed-Use Core. The sidewalk may be a combination of public and private property.
- Reference Parking section for requirements.
- See General Standards for requirements of frontage elements.

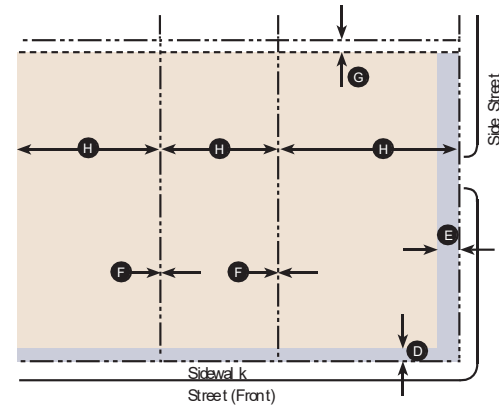
**General Table NOTES:**

- All buildings shall have a primary pedestrian entrance along the front façade.
- Loading docks and other service entries shall not be located on Mixed-use Core frontages.

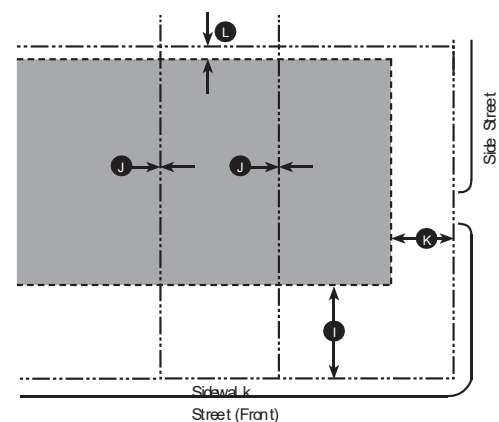
**1. BUILDING HEIGHT**



**2. BUILDING PLACEMENT**



**3. PARKING LOCATION & FRONTAGE ELEMENTS**



**Key**

- Property Line
- Setback Line
- Build-to Zone
- Potential Building Area (in addition to Build to Zone)
- Parking Area



**Sec. 24-3826. Mixed-use Core Frontage Standards**

**Building Heights**

<b>A</b>	Primary Street Building Height <sup>1</sup>	75' max <sup>1,2</sup>
<b>A</b>	Secondary Street Building Height <sup>1</sup>	65' max <sup>1,2</sup>
<b>A</b>	Tertiary Street Building Height <sup>1</sup>	60' max <sup>1,2</sup>
<b>B</b>	First Floor Height (floor to floor)	13' minimum
<b>C</b>	Ground Finished Floor above sidewalk or finished grade	Commercial: 0' min Residential: 2' min

**Building Placement**

<b>D</b>	Primary Street Build-to Zone <sup>3</sup>	0' minimum to 10' maximum <sup>3</sup>
<b>E</b>	Secondary Street Build-to Zone <sup>3</sup>	0' minimum to 10' maximum <sup>3</sup>
<b>F</b>	Interior Side Yard Setback	0' minimum
	Tertiary Street Build-to Zone	0' minimum to 10' maximum
<b>G</b>	Alley or Rear Setback	5' minimum
	Lot Coverage	90% maximum

**Frontage Built-Out**

<b>H</b>	Building Front	80% minimum
	Building Side (street)	40% minimum
	Building Rear (street)	60% minimum

**Parking Location**

<b>I</b>	Front Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>J</b>	Interior Side Yard Setback <sup>4</sup>	0' minimum <sup>4</sup>
<b>K</b>	Street Side Yard Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>L</b>	Rear Setback (lot) <sup>4</sup>	5' minimum <sup>4</sup>
	Rear Setback (alley) <sup>4</sup>	0' minimum <sup>4</sup>
	Rear Setback (Street) <sup>4</sup>	30' minimum <sup>4</sup>

**Frontage Elements**

Allowed Frontage Elements <sup>5</sup>	Forecourt, Stoop, Shopfront, Gallery, Arcade / Colonnade <sup>5</sup>
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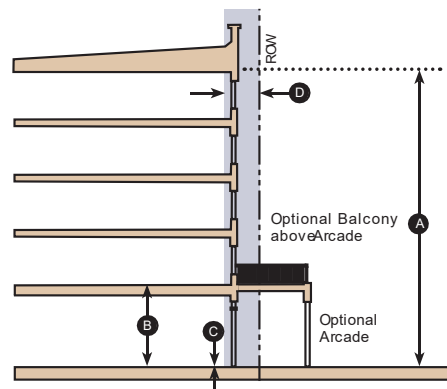
**NOTES:**

1. Allowable building height determined based on street designation where front of building is located.
2. Towers, cupolas, and other architectural rooftop features with a footprint smaller than 15' by 15' may extend up to 20' above the designated height limit.
3. A minimum front 13' sidewalk shall be provided in the Mixed-Use Core. The sidewalk may be a combination of public and private property.
4. Reference Parking section for requirements.
5. See General Standards for requirements of frontage elements.

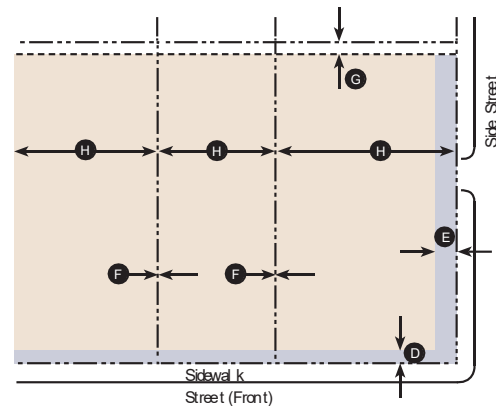
**General Table NOTES:**

- All buildings shall have a primary pedestrian entrance along the front façade.
- Loading docks and other service entries shall not be located on Mixed-use Core frontages.

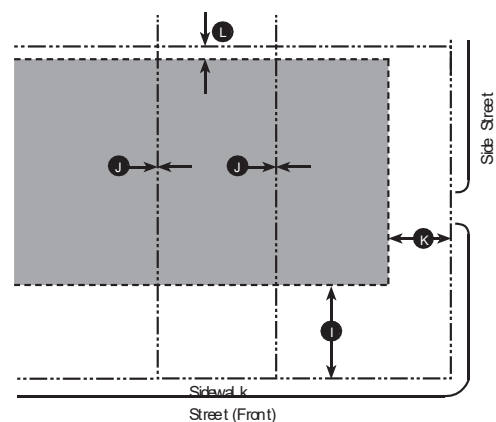
**1. BUILDING HEIGHT**



**2. BUILDING PLACEMENT**



**3. PARKING LOCATION & FRONTAGE ELEMENTS**



**Key**

- - - - - Property Line
- - - - - Setback Line
- █ Build-to Zone
- █ Potential Building Area (in addition to Build to Zone)
- █ Parking Area

**Sec. 24-3827. Walkable Corridor Frontage Standards**

Building Heights		
<b>A</b>	Primary Street Building Height <sup>1</sup>	60' max <sup>1,2</sup>
<b>A</b>	Secondary Street Building Height <sup>1</sup>	60' max <sup>1,2</sup>
<b>A</b>	Tertiary Street Building Height <sup>1</sup>	55' max <sup>1,2</sup>
<b>B</b>	First Floor Height (floor to floor)	13' minimum
<b>C</b>	Ground Finished Floor above sidewalk or finished grade	Commercial: 0' min Residential: 2' min
Building Placement		
<b>D</b>	Primary Street Build-to Zone <sup>3</sup>	15' minimum to 25' maximum <sup>3</sup>
<b>E</b>	Secondary Street Build-to Zone <sup>3</sup>	0' minimum to 15' maximum <sup>3</sup>
<b>F</b>	Interior Side Yard Setback	0' minimum
	Tertiary Street Build-to Zone	6' minimum to 18' maximum
<b>G</b>	Alley or Rear Setback	5' minimum
	Lot Coverage	75% maximum
Frontage Built-Out		
<b>H</b>	Building Front	60% minimum
	Building Side (street)	30% minimum
	Building Rear (street)	40% minimum
Parking Location		
<b>I</b>	Front Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>J</b>	Interior Side Yard Setback <sup>4</sup>	0' minimum <sup>4</sup>
<b>K</b>	Street Side Yard Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>L</b>	Rear Setback (lot) <sup>4</sup>	5' minimum <sup>4</sup>
	Rear Setback (alley) <sup>4</sup>	0' minimum <sup>4</sup>
	Rear Setback (Street) <sup>4</sup>	20' minimum <sup>4</sup>
Frontage Elements		
	Allowed Frontage Elements <sup>5</sup>	Terrace or Lightwell, Forecourt, Stoop, Shopfront, Gallery, Arcade/Colonnade <sup>5</sup>

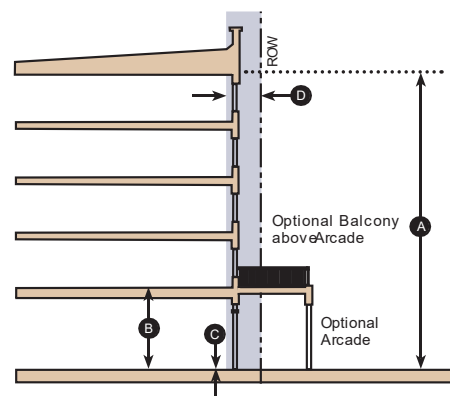
**NOTES:**

1. Allowable building height determined based on street designation where front of building is located.
2. Towers, cupolas, and other architectural rooftop features with a footprint smaller than 15' by 15' may extend up to 20' above the designated height limit.
3. A minimum front 13' sidewalk shall be provided in the Mixed-Use Core. The sidewalk may be a combination of public and private property.
4. Reference Parking section for requirements.
5. See General Standards for requirements of frontage elements.

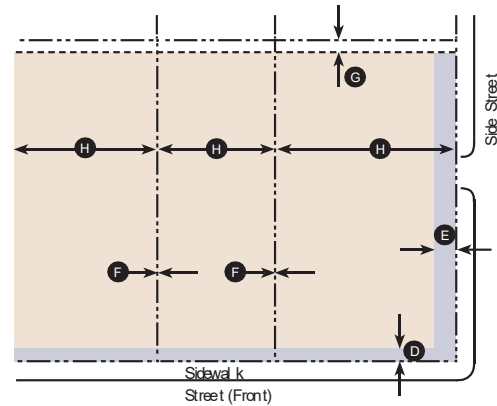
**General Table NOTES:**

- All buildings shall have a primary pedestrian entrance along the front façade.
- Loading docks and other service entries shall not be located on Walkable Corridor frontages.

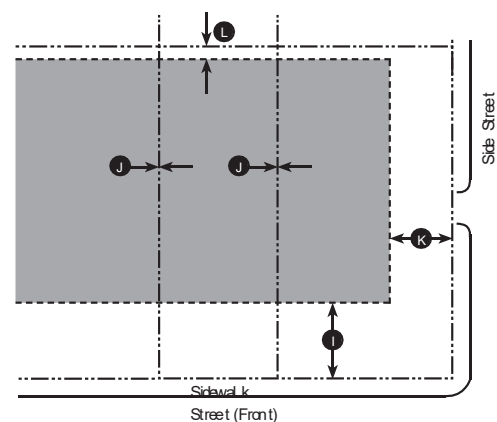
**1. BUILDING HEIGHT**



**2. BUILDING PLACEMENT**



**3. PARKING LOCATION & FRONTAGE ELEMENTS**



**Key**

- - - - - Property Line
- - - - - Setback Line
- Build-to Zone
- Potential Building Area (in addition to Build to Zone)
- Parking Area

**Sec. 24-3828. Walkable Center Frontage Standards**

**Building Heights**

<b>A</b>	Primary Street Building Height <sup>1</sup>	60' max <sup>1,2</sup>
<b>A</b>	Secondary Street Building Height <sup>1</sup>	60' max <sup>1,2</sup>
<b>A</b>	Tertiary Street Building Height <sup>1</sup>	55' max <sup>1,2</sup>
<b>B</b>	First Floor Height (floor to floor)	13' minimum
<b>C</b>	Ground Finished Floor above sidewalk or finished grade	Commercial: 0' min Residential: 2' min

**Building Placement**

<b>D</b>	Primary Street Build-to Zone <sup>3</sup>	0' minimum to 10' maximum <sup>3</sup>
<b>E</b>	Secondary Street Build-to Zone <sup>3</sup>	0' minimum to 12' maximum <sup>3</sup>
<b>F</b>	Interior Side Yard Setback	0' minimum
	Tertiary Street Build-to Zone	6' minimum to 18' maximum
<b>G</b>	Alley or Rear Setback	5' minimum
	Lot Coverage	75% maximum

**Frontage Built-Out**

<b>H</b>	Building Front	60% minimum
	Building Side (street)	30% minimum
	Building Rear (street)	40% minimum

**Parking Location**

<b>I</b>	Front Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>J</b>	Interior Side Yard Setback <sup>4</sup>	0' minimum <sup>4</sup>
<b>K</b>	Street Side Yard Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>L</b>	Rear Setback (lot) <sup>4</sup>	5' minimum <sup>4</sup>
	Rear Setback (alley) <sup>4</sup>	0' minimum <sup>4</sup>
	Rear Setback (Street) <sup>4</sup>	20' minimum <sup>4</sup>

**Frontage Elements**

Allowed Frontage Elements <sup>5</sup>	Terrace or Lightwell, Forecourt, Stoop, Shopfront, Gallery, Arcade/Colonnade <sup>5</sup>
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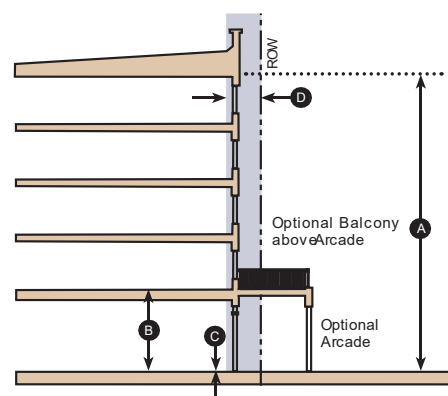
**NOTES:**

1. Allowable building height determined based on street designation where front of building is located.
2. Towers, cupolas, and other architectural rooftop features with a footprint smaller than 15' by 15' may extend up to 20' above the designated height limit.
3. A minimum front 13' sidewalk shall be provided in the Mixed-Use Core. The sidewalk may be a combination of public and private property.
4. Reference Parking section for requirements.
5. See General Standards for requirements of frontage elements.

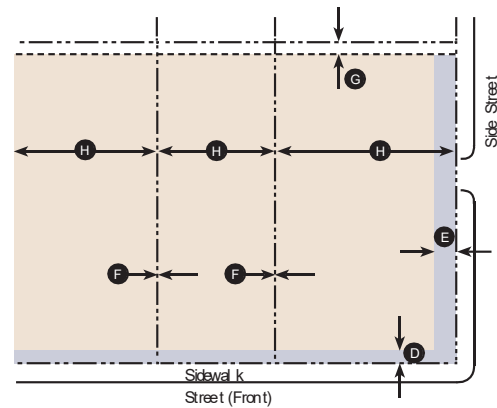
**General Table NOTES:**

- All buildings shall have a primary pedestrian entrance along the front façade.
- Loading docks and other service entries shall not be located on Walkable Center frontages.

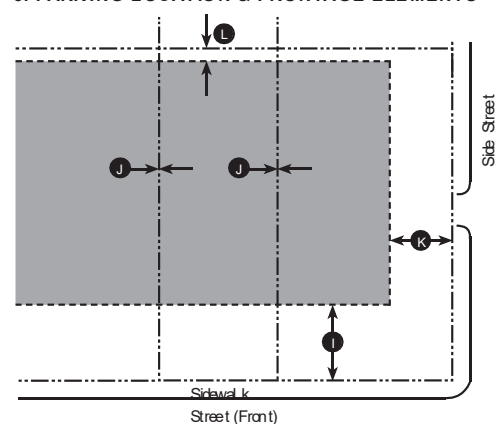
**1. BUILDING HEIGHT**



**2. BUILDING PLACEMENT**



**3. PARKING LOCATION & FRONTAGE ELEMENTS**



**Key**

--- Property Line	Build-to Zone
--- Setback Line	Potential Building Area (in addition to Build-to Zone)
	Parking Area

**Sec. 24-3829. Neighborhood General Frontage Standards**

**Building Heights**

<b>A</b>	Primary Street Building Height <sup>1</sup>	55' max <sup>1,2</sup>
<b>A</b>	Secondary Street Building Height <sup>1</sup>	55' max <sup>1,2</sup>
<b>A</b>	Tertiary Street Building Height <sup>1</sup>	50' max <sup>1,2</sup>
<b>B</b>	First Floor Height (floor to floor)	13' minimum
<b>C</b>	Ground Finished Floor above sidewalk or finished grade	Commercial: 0' min Residential: 2' min

**Building Placement**

<b>D</b>	Primary Street Build-to Zone	0' minimum to 12' maximum
<b>E</b>	Secondary Street Build-to Zone	0' minimum to 18' maximum
<b>F</b>	Interior Side Yard Setback	0' minimum
	Tertiary Street Build-to Zone	6' minimum to 18' maximum
<b>G</b>	Alley or Rear Setback	5' minimum
	Lot Coverage	70% maximum

**Frontage Built-Out**

<b>H</b>	Building Front	60% minimum
	Building Side (street)	30% minimum
	Building Rear (street)	40% minimum

**Parking Location**

<b>I</b>	Front Setback <sup>4</sup>	20' minimum <sup>4</sup>
<b>J</b>	Interior Side Yard Setback <sup>4</sup>	0' minimum <sup>4</sup>
<b>K</b>	Street Side Yard Setback <sup>4</sup>	20' minimum <sup>4</sup>
<b>L</b>	Rear Setback (lot) <sup>4</sup>	5' minimum <sup>4</sup>
	Rear Setback (alley) <sup>4</sup>	3' minimum <sup>4</sup>
	Rear Setback (Street) <sup>4</sup>	20' minimum <sup>4</sup>

**Frontage Elements**

<sup>M</sup> Allowed Frontage Elements <sup>4</sup>	Porch, Terrace or Lightwell, Forecourt, Stoop, Shopfront <sup>4</sup>
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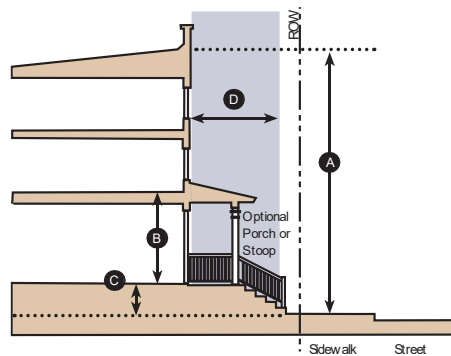
**NOTES:**

1. Allowable building height determined based on street designation where front of building is located.
2. Towers, cupolas, and other architectural rooftop features with a footprint smaller than 15' by 15' may extend up to 20' above the designated height limit.
3. Reference Parking section for requirements.
4. See General Standards for requirements of frontage elements.

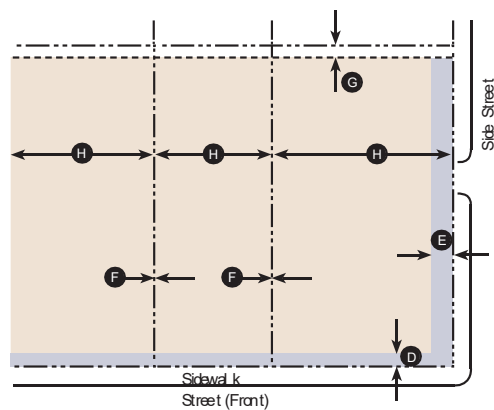
**General Table NOTES:**

- All buildings shall have a primary pedestrian entrance along the front façade.

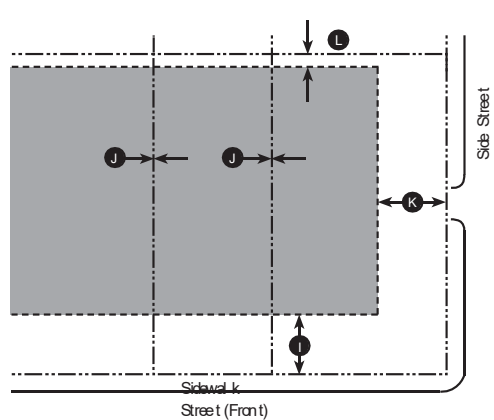
**1. BUILDING HEIGHT**



**2. BUILDING PLACEMENT**



**3. PARKING LOCATION & FRONTAGE ELEMENTS**



**Key**

- - - - - Property Line
- - - - - Setback Line
- █ Build-to Zone
- █ Potential Building Area (in addition to Build to Zone)
- █ Parking Area

**Sec. 24-3830. Connected Edge Frontage Standards**

**Building Heights**

<b>A</b>	Primary Street Building Height <sup>1</sup>	40' max <sup>1,2</sup>
<b>A</b>	Secondary Street Building Height <sup>1</sup>	35' max <sup>1,2</sup>
<b>A</b>	Tertiary Street Building Height <sup>1</sup>	35' max <sup>1,2</sup>
<b>B</b>	First Floor Height (floor to floor)	13' minimum
<b>C</b>	Ground Finished Floor above sidewalk or finished grade	Residential: 2' min

**Building Placement**

<b>D</b>	Primary Street Build-to Zone	10' minimum setback
<b>F</b>	Interior Side Yard Setback	5' minimum
	Tertiary Street Build-to Zone	12' minimum setback
<b>G</b>	Alley or Rear Setback	5' minimum
	Lot Coverage	60% maximum

**Frontage Built-Out**

<b>H</b>	Building Front	40% minimum
	Building Side (street)	20% minimum
	Building Rear (street)	N/A

**Parking Location**

<b>I</b>	Front Setback <sup>3</sup>	24' minimum <sup>3</sup>
<b>J</b>	Interior Side Yard Setback <sup>3</sup>	0' minimum <sup>3</sup>
<b>K</b>	Street Side Yard Setback <sup>3</sup>	20' minimum <sup>3</sup>
<b>L</b>	Rear Setback (lot) <sup>3</sup>	5' minimum <sup>3</sup>
	Rear Setback (alley) <sup>3</sup>	3' minimum <sup>3</sup>
	Rear Setback (Street) <sup>3</sup>	20' minimum <sup>3</sup>

**Frontage Elements**

Allowed Frontage Elements <sup>4</sup>	Porch, Stoop, Common Yard <sup>4</sup>
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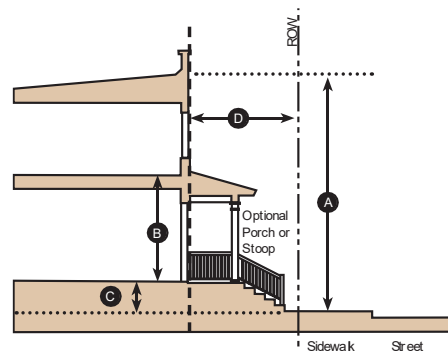
**NOTES:**

1. Allowable building height determined based on street designation where front of building is located.
2. Towers, cupolas, and other architectural rooftop features with a footprint smaller than 15' by 15' may extend up to 20' above the designated height limit.
3. Reference Parking section for requirements.
4. See General Standards for requirements of frontage elements.

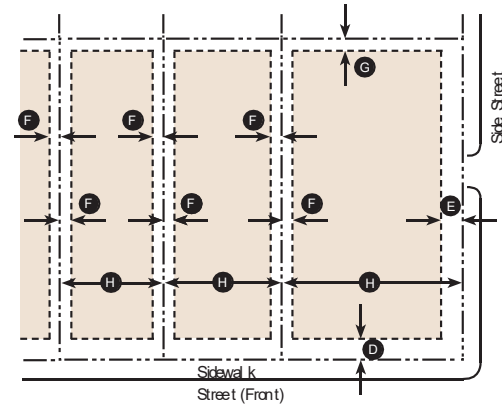
**General Table NOTES:**

- All buildings shall have a primary pedestrian entrance along the front façade.

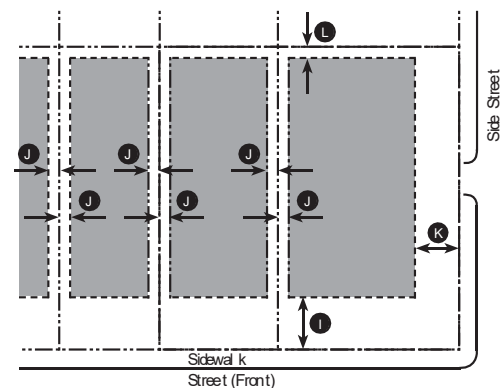
**1. BUILDING HEIGHT**



**2. BUILDING PLACEMENT**



**3. PARKING LOCATION & FRONTAGE ELEMENTS**



**Key**

- Property Line
- Setback Line
- Potential Building Area (in addition to Build to Zone)
- Parking Area



**Sec. 24-3831. Highway Edge Frontage Standards**

**Building Heights**

<b>A</b>	Primary Street Building Height <sup>1</sup>	165' max <sup>1,2</sup>
<b>A</b>	Secondary Street Building Height <sub>1</sub>	165' max <sup>1,2</sup>
<b>A</b>	Tertiary Street Building Height <sup>1</sup>	110' max <sup>1,2</sup>
<b>B</b>	First Floor Height (floor to floor)	13' minimum
<b>C</b>	Ground Finished Floor above sidewalk or finished grade	Commercial: 0' min Residential: 0' min

**Building Placement**

<b>D</b>	Primary Street Build-to Zone <sup>3</sup>	0' minimum to 8' maximum <sup>3</sup>
<b>E</b>	Secondary Street Build-to Zone <sup>3</sup>	0' minimum to 10' maximum <sup>3</sup>
<b>F</b>	Interior Side Yard Setback	0' minimum
	Tertiary Street Build-to Zone	0' minimum to 10' maximum
<b>G</b>	Alley or Rear Setback	5' minimum
	Lot Coverage	90% maximum

**Frontage Built-Out**

<b>H</b>	Building Front	80% minimum
	Building Side (street)	40% minimum
	Building Rear (street)	60% minimum

**Parking Location**

<b>I</b>	Front Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>J</b>	Interior Side Yard Setback <sup>4</sup>	0' minimum <sup>4</sup>
<b>K</b>	Street Side Yard Setback <sup>4</sup>	30' minimum <sup>4</sup>
<b>L</b>	Rear Setback (lot) <sup>4</sup>	5' minimum <sup>4</sup>
	Rear Setback (alley) <sup>4</sup>	0' minimum <sup>4</sup>
	Rear Setback (Street) <sup>4</sup>	30' minimum <sup>4</sup>

**Frontage Elements**

Allowed Frontage Elements <sup>5</sup>	Terrace or Lightwell, Forecourt, Stoop, Shopfront, Gallery, Arcade / Colonnade <sup>5</sup>
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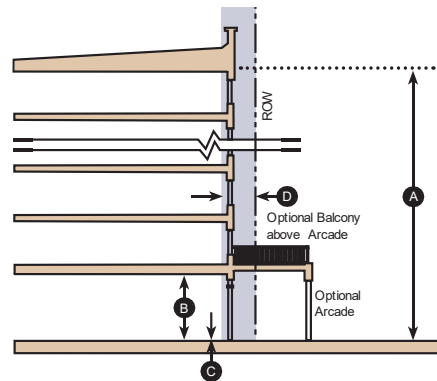
**NOTES:**

1. Allowable building height determined based on street designation where front of building is located.
2. Towers, cupolas, and other architectural rooftop features with a footprint smaller than 15' by 15' may extend up to 20' above the designated height limit.
3. A minimum front 13' sidewalk shall be provided in the Mixed-Use Core. The sidewalk may be a combination of public and private property.
4. Reference Parking section for requirements.
5. See General Standards for requirements of frontage elements.

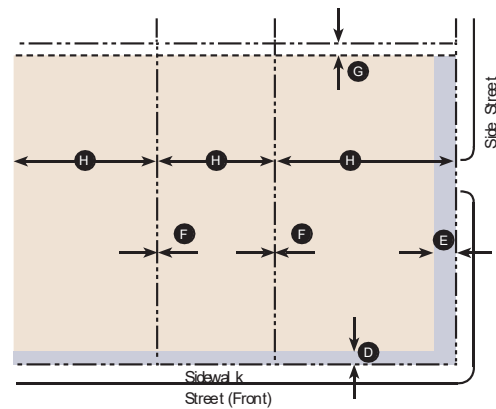
**General Table NOTES:**

- All buildings shall have a primary pedestrian entrance along the front façade.
- Loading docks and other service entries shall not be located on Mixed-use Core frontages.

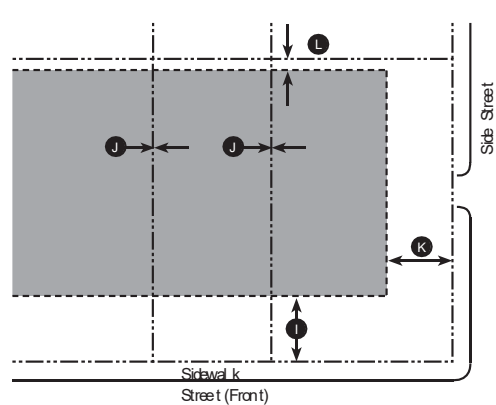
**1. BUILDING HEIGHT**



**2. BUILDING PLACEMENT**



**3. PARKING LOCATION & FRONTAGE ELEMENTS**



**Key**

- - - - - Property Line
- - - - - Setback Line
- █ Build-to Zone
- █ Potential Building Area (in addition to Build to Zone)
- █ Parking Area

**Sec. 24-3832. Use Standards**

Principal, accessory, and temporary uses are allowed in each of the development areas in accordance with Table 3832: FBA-O District Use Table.

<b>Table 3832: FBA-O District Use Table</b>								
R=Permitted by right   C=Allowed subject to conditional use permit P=Allowed subject to provisional use permit   - = prohibited T=Allowed subject to temporary use permit								
Use Category	Use Type	Mixed-use Corridor	Mixed-use Core	Walkable Corridor	Walkable Center	Neighborhood General	Connected Edge	Highway Edge
<b>Agricultural Uses</b>								
<b>Horticulture</b>	All Horticulture uses	-	-	-	-	-	-	-
<b>Animal Husbandry</b>	All Animal Husbandry uses	-	-	-	-	-	-	-
<b>Agriculture Support and Services (Directly Related)</b>	Agri-education	-	-	-	-	-	-	-
	Agri-entertainment	-	-	-	-	-	-	-
	Agricultural processing	-	-	-	-	-	-	-
	Agribusiness	-	-	-	-	-	-	-
	Equestrian facility	-	-	-	-	-	-	-
	Farm or limited production of alcoholic beverages	-	-	-	-	-	-	-
	Nursery, production	-	-	-	-	-	-	P
	Produce market	-	-	-	-	-	-	-
<b>Agriculture Support and Services (Not Directly Related)</b>	Agricultural research facility	-	-	-	-	-	-	-
	Distribution hub for agricultural products	-	-	-	-	-	-	-
	Farm machinery sales, rental, and service	-	-	-	-	-	-	-
	Stockyard or Slaughterhouse	-	-	-	-	-	-	-
<b>Silviculture</b>	All Silviculture uses	-	-	-	-	-	-	-
<b>Residential Uses</b>								
<b>Household Living</b>	Dwelling, duplex	R	R	R	R	R	R	-
	Dwelling, live/work	R	R	R	R	R	R	R
	Dwelling, mansion apartment	R	R	R	R	R	R	R
	Dwelling, manufactured home	-	-	-	-	-	-	-
	Dwelling, multi-family	R	R	R	R	R	R	R
	Dwelling, single-family attached	R	R	R	R	R	R	-
	Dwelling, single-family detached	-	-	-	-	R	R	-
	Dwelling, townhouse	R	R	R	R	R	R	R
	Dwelling, upper story	R	R	R	R	R	-	R
<b>Group Living</b>	Boardinghouse	C	C	C	C		C	C
	Children’s residential facility	-	-	R	R	R	R	-
	Dormitory	-	-	-	-	-	-	-
<b>Public, Civic, and Institutional Uses</b>								
<b>Community Services</b>	Auditorium, conference and convention center	-	-	-	-	-	-	-
	Club or lodge	R	R	R	R	-	-	-
	Community center, private	R	R	R	R	R	R	R
	Community center, public	R	R	R	R	R	R	R

**Table 3832: FBA-O District Use Table**

**R**=Permitted by right | **C**=Allowed subject to conditional use permit  
**P**=Allowed subject to provisional use permit | - = prohibited  
**T**=Allowed subject to temporary use permit

Use Category	Use Type	Mixed-use Corridor	Mixed-use Core	Walkable Corridor	Walkable Center	Neighborhood General	Connected Edge	Highway Edge
	Cultural facility	R	R	R	R	R	R	R
	Donation center	-	-	-	-	-	-	-
	Radio or television station	-	-	-	-	-	-	R
	Religious institution	R	R	R	R	R	R	R
<b>Day Care</b>	Adult day care center	R	R	R	R	R	-	-
	Child care center	R	R	R	R	R	-	-
<b>Educational Facilities</b>	College or university	-	-	-	-	-	-	-
	School, elementary or secondary	R	R	R	R	R	R	-
	Vocational or trade school	-	-	-	-	-	-	R
<b>Funeral and Mortuary Services</b>	Crematory	-	-	-	-	-	-	-
	Funeral home	R	R	R	R	-	-	R
<b>Government Facilities</b>	Correctional facility	-	-	-	-	-	-	-
	Government maintenance, storage, or distribution facility	-	-	-	-	-	-	-
	Government office	R	R	R	R	R	R	R
	Police, fire, or EMS facility	R	R	R	R	R	R	R
	All other government facilities	R	R	R	R	R	R	R
<b>Health Care Facilities</b>	Assisted living facility	R	R	R	R	R	R	R
	Continuing care retirement community	R	R	R	R	-	-	R
	Hospice facility	R	R	R	R	-	-	-
	Hospital	-	-	-	-	-	-	R
	Medical treatment facility	R	R	R	R	-	-	R
	Nursing home	R	R	R	R	R	R	R
<b>Parks and Open Areas</b>	Arboretum or botanical garden	R	R	R	R	R	R	R
	Cemetery	-	-	-	-	-	-	-
	Community garden	R	R	R	R	R	R	R
	Fairgrounds	-	-	-	-	-	-	-
	Public park	R	R	R	R	R	R	R
<b>Transportation</b>	Airport	-	-	-	-	-	-	-
	Helicopter landing facility	-	-	-	-	-	-	P
	Passenger terminal, surface transportation	R	R	R	R	R	R	R
<b>Utilities</b>	Solar array	-	-	-	-	-	-	-
	Utility, major	-	-	-	-	-	-	-
	Utility, minor	R	R	R	R	R	R	R
	Wind energy facility, large	-	-	-	-	-	-	-
	Wireless communications short structure or co-location	R	R	R	R	R	R	R
	Wireless communications tower, freestanding	P	P	P	P	P	P	P

<b>Table 3832: FBA-O District Use Table</b> R=Permitted by right   C=Allowed subject to conditional use permit P=Allowed subject to provisional use permit   - = prohibited T=Allowed subject to temporary use permit								
Use Category	Use Type	Mixed-use Corridor	Mixed-use Core	Walkable Corridor	Walkable Center	Neighborhood General	Connected Edge	Highway Edge
<b>Commercial Uses</b>								
<b>Adult Uses</b>	All Adult Uses	-	-	-	-	-	-	-
<b>Animal Care</b>	Animal grooming	R	R	R	R	-	-	R
	Animal shelter	-	-	-	-	-	-	-
	Kennel	-	-	-	-	-	-	-
	Veterinary hospital or clinic	R	R	-	-	-	-	R
<b>Eating Establishments</b>	Microbrewery	R	R	R	R	R	-	R
	Restaurant	R	R	R	R	R	-	R
	Restaurant, drive-through	P	P	P	P	-	-	R
	Specialty eating establishment	R	R	R	R	-	-	R
<b>Offices</b>	Business and sales	R	R	R	R	P	P	R
	Business school	R	R	R	R	-	-	R
	Business training and conference facility	R	R	R	R	-	-	R
	Data center	-	-	-	-	-	-	R
	Professional services	R	R	R	R	R	R	R
<b>Parking, Commercial</b>	Parking lot	R	R	R	R	R	R	R
	Parking structure	R	R	R	R	R	R	R
<b>Recreation and Entertainment, Indoor</b>	Fitness center	R	R	R	R	R	-	R
	Shooting range, indoor	-	-	-	-	-	-	-
	Theater	R	R	R	R	-	-	R
	Indoor Recreation and Entertainment not elsewhere listed	R	R	R	R	-	-	R
<b>Recreation and Entertainment, Outdoor</b>	Amusement park, sports park, or waterpark	-	-	-	-	-	-	-
	Arena, amphitheater, or stadium	-	-	-	-	-	-	-
	Golf course	-	-	-	-	-	-	-
	Marina	-	-	-	-	-	-	-
	Racetrack, outdoor	-	-	-	-	-	-	-
	Shooting range, outdoor	-	-	-	-	-	-	-
	Outdoor Recreation and Entertainment not elsewhere listed	-	-	-	-	-	-	-
<b>Retail Sales and Services</b>	Aircraft and aircraft parts sales	-	-	-	-	-	-	-
	Alternative lending institution	C	C	C	C	-	-	C
	Artist studio	R	R	R	R	R	R	R
	Automated teller machine (ATM)	R	R	R	R	R	R	R
	Auction house	-	-	-	-	-	-	-
	Convenience store	R	R	R	R	R	R	R
	Drive-through ice machine	-	-	-	-	-	-	R
	Drug store or pharmacy	R	R	R	R	-	-	R
	Farmers market	R	R	R	R	R	R	R

**Table 3832: FBA-O District Use Table**

**R**=Permitted by right | **C**=Allowed subject to conditional use permit  
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Use Category	Use Type	Mixed-use Corridor	Mixed-use Core	Walkable Corridor	Walkable Center	Neighborhood General	Connected Edge	Highway Edge
	Financial institution	R	R	R	R	-	-	R
	Grocery store	R	R	R	R	-	-	R
	Laundromat	R	R	-	-	-	-	R
	Personal services establishment	R	R	R	R	-	-	R
	Repair establishment	-	-	-	-	-	-	R
	Retail sales establishment, not elsewhere listed	R	R	R	R	P	-	R
	Sign printing and painting	R	R	R	R	-	-	R
<b>Vehicle Sales and Services</b>	Automobile filling station (fuel only)	P	P	P	P	-	-	P
	Automobile rentals	R	R	-	-	-	-	R
	Automobile sales	-	-	-	-	-	-	R
	Automotive painting and body shop	-	-	-	-	-	-	-
	Automotive parts and installation and minor servicing	-	-	-	-	-	-	R
	Automotive repair	-	-	-	-	-	-	R
	Boat and marine rental, sales, and service	-	-	-	-	-	-	-
	Car wash or auto detailing	-	-	-	-	-	-	R
	Commercial fuel depot	-	-	-	-	-	-	-
	Commercial vehicle repair and maintenance	-	-	-	-	-	-	-
	Commercial vehicle sales and rentals	-	-	-	-	-	-	-
	Fleet terminal	-	-	-	-	-	-	-
Towing or wrecker service	-	-	-	-	-	-	-	
<b>Visitor Accommodations</b>	Bed and breakfast inn	R	R	R	R	R	R	R
	Campground	-	-	-	-	-	-	-
	Hotel or Motel	R	R	R	R	-	P	R
<b>Industrial Uses</b>								
<b>Extractive Industry</b>	All Extractive Industry uses	-	-	-	-	-	-	-
<b>Industrial Services</b>	Contractor services	-	-	-	-	-	-	-
	Fuel oil or bottled gas distributor	-	-	-	-	-	-	-
	General industrial service and repair	-	-	-	-	-	-	-
	Heavy equipment sales, rental, and service	-	-	-	-	-	-	-
	Laundry, dry cleaning, and carpet cleaning plants	-	-	-	-	-	-	-
	Manufactured home and prefabricated building sales	-	-	-	-	-	-	-
	Research and development facility	-	-	-	-	-	-	-
<b>Manufacturing and Production</b>	Manufacturing, artisan	R	R	R	R	-	-	R
	Manufacturing, heavy	-	-	-	-	-	-	-
	Manufacturing, light	-	-	-	-	-	-	R
<b>Warehouse and Freight Movement</b>	Cold storage plant	-	-	-	-	-	-	-
	Mini-warehouse	-	-	-	-	-	-	P

**Table 3832: FBA-O District Use Table**

**R**=Permitted by right | **C**=Allowed subject to conditional use permit  
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**T**=Allowed subject to temporary use permit

Use Category	Use Type	District						
		Mixed-use Corridor	Mixed-use Core	Walkable Corridor	Walkable Center	Neighborhood General	Connected Edge	Highway Edge
	Outdoor storage (as a principal use)	-	-	-	-	-	-	-
	Self-service storage facility	-	-	-	-	-	-	-
	Truck or freight terminal	-	-	-	-	-	-	-
	Warehouse (distribution)	-	-	-	-	-	-	-
	Warehouse (storage)	-	-	-	-	-	-	-
<b>Waste-Related Services</b>	Landfill, construction and demolition debris	-	-	-	-	-	-	-
	Landfill, sanitary	-	-	-	-	-	-	-
	Recycling collection center	-	-	-	-	-	-	-
	Recycling processing center	-	-	-	-	-	-	-
	Salvage and junkyard	-	-	-	-	-	-	-
	Solid waste transfer station	-	-	-	-	-	-	-
	Waste composting	-	-	-	-	-	-	-
<b>Wholesale Sales</b>	All wholesale sales	-	-	-	-	-	-	-
<b>Accessory Uses/Structures</b>								
	Accessory dwelling unit	R	R	R	R	R	R	R
	Agritourism activity	-	-	-	-	-	-	-
	Antenna	R	R	R	R	R	R	R
	Automated teller machine (ATM) (as an accessory use)	R	R	R	R	R	R	R
	Automatic car wash	-	-	-	-	-	-	R
	Bicycle parking rack	R	R	R	R	R	R	R
	Bike share station	R	R	R	R	R	R	R
	Caretaker dwelling unit	R	R	R	R	R	R	R
	Child care center (as an accessory use)	R	R	R	R	R	R	R
	Community garden (as an accessory use)	R	R	R	R	R	R	R
	Composting, small scale	R	R	R	R	R	R	R
	Crematory chamber (as accessory to a funeral home or a veterinary hospital or clinic)	R	R	R	R	R	R	R
	Donation box	R	R	R	R	R	R	R
	Drive-through facility	P	P	P	P	-	-	P
	Electric vehicle (EV) level 1, 2, or 3 charging station	R	R	R	R	R	R	R
	Family day home (as accessory to a dwelling)	R	R	R	R	R	R	R
	Greenhouse	-	-	-	-	R	R	R
	Green roof	R	R	R	R	R	R	R
	Helicopter landing facility (as an accessory use)	-	-	-	-	-	-	R
	Home garden (as accessory to a dwelling)	R	R	R	R	R	R	R
	Home occupation	R	R	R	R	R	R	R
	Keeping of animals outside a dwelling (as accessory to a dwelling)	R	R	R	R	R	R	R
	Limited fuel oil or bottled gas distribution	-	-	-	-	-	-	-

**Table 3832: FBA-O District Use Table**

**R**=Permitted by right | **C**=Allowed subject to conditional use permit  
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**T**=Allowed subject to temporary use permit

Use Category	Use Type	Mixed-use Corridor	Mixed-use Core	Walkable Corridor	Walkable Center	Neighborhood General	Connected Edge	Highway Edge
Outdoor display of merchandise (as accessory to a Retail Sales and Services use)		R	R	R	R	-	R	R
Outdoor seating (as accessory to an Eating Establishments use)		R	R	R	R	R	R	R
Outdoor storage		-	-	-	-	-	-	-
Parking of trucks and commercial vehicles (as accessory to a dwelling)		-	-	-	-	-	-	-
Parking facility (as accessory use)		R	R	R	R	R	R	R
Produce stand (as accessory to an Horticulture use or a community garden)		R	R	R	R	R	R	R
Rainwater cistern or barrel		R	R	R	R	R	R	R
Recycling and refuse collection area, outdoor		R	R	R	R	R	R	R
Retail sales (as accessory to an Industrial use)		R	R	R	R	R	R	R
Sand and gravel washing and grading plant (as accessory to an Extractive Industry use)		-	-	-	-	-	-	-
Short-term rental		R	R	R	R	R	R	R
Solar energy equipment		R	R	R	R	R	R	R
Swimming pool (as accessory to a dwelling)		R	R	R	R	R	R	R
Tasting and retail sales (as accessory to winery, brewery, or distillery)		-	-	-	-	-	-	-
Truck rental (as accessory to retail sales, mini-warehouse, or self-service storage)		-	-	-	-	-	-	R
Underground storage tank		R	R	R	R	-	-	R
Vending machine		R	R	R	R	R	R	R
Wind energy system (small)		R	R	R	R	R	R	R
<b>Temporary Uses/Structures</b>								
Construction-related building, structure, or use		T	T	T	T	T	T	T
Family healthcare home, temporary		T	T	T	T	T	T	T
Farmers' market, temporary		T	T	T	T	T	T	T
Flea market, temporary		T	T	-	-	T	T	T
Food truck		R	R	R	R	R	R	R
Garage/yard sale		R	R	R	R	R	R	R
Model sales home/unit		T	T	T	T	T	T	T
Portable storage		R	R	R	R	R	R	R
Recyclables collection point, temporary		T	T	T	T	T	T	T
Sawmill, temporary		-	-	-	-	-	-	-
Seasonal decorations sales and commercial display		R	R	-	-	R	R	R
Special event		R	R	-	-	R	R	R
Temporary produce stand		-	-	-	-	-	R	R
Temporary use of an accessory structure as a principal dwelling after a catastrophe		R	R	R	R	R	R	R
Temporary use of a factory-fabricated transportable building		T	T	T	T	T	T	T



**Sec. 24-3833. *Parking Standards: Purpose***

The purpose and intent of the parking standards is to encourage a balance between pedestrian-oriented development and necessary car storage, so that neither more nor less parking than is needed is developed.

**Sec. 24-3834. *Parking Standards: Minimum Parking Standards***

- A. A minimum number of parking spaces shall be provided for each use in accordance with Table 3834: Minimum Parking Spaces.

<b>Table 3834: Minimum Parking Spaces</b>	
<b>Use</b>	<b>Minimum Number of Parking Spaces</b>
Residential, Multi-family	1.5 spaces per dwelling unit
Residential, Single-Family	1 space per dwelling unit
Hotel or Motel	1 space per bedroom
Office Uses	3 spaces per 1,000 sf of gross floor area
Retail Sales and Service Uses	
Industrial Uses	
Public, Civic, and Institutional Use	As determined by the Planning Director

- B. The Planning Director may approve a reduction of up to 30 percent of the minimum parking standards in subsection A above, based on a parking study submitted by the applicant demonstrating that parking demand is not needed or may be met by sharing parking with other existing uses.
- C. Parking spaces required by subsection A above, may include on-street parking spaces adjacent to the property. The on-street parking shall be within 200 feet of the front of the building for which the parking is to serve.

**Sec. 24-3835. *Parking Standards: Shared Parking Standards***

- A. Shared parking is encouraged in all subdistricts for more efficient parking solutions. The amount of parking required is calculated by adding the total number of spaces required by Table 3834: Minimum Parking Spaces, for each separate use and dividing by the appropriate factor from Table 3835: Shared Parking Factors, based on the uses proposed to share parking. The shared parking calculations shall be documented in a shared parking study prepared by a professional transportation engineer approved by the Planning Director. For example, consider a residential use that requires ten spaces and an office use that requires twelve spaces. If the uses shared parking, the sum of the separate minimum parking requirements, 22 spaces, would be divided by the sharing factor of 1.4, resulting in a shared parking requirement of 16 spaces. When more than two uses share parking, the lowest shared parking factor shall apply.

<b>Table 3835: Shared Parking Factors</b>				
<b>Use Sharing Parking</b>	<b>Use Sharing Parking</b>			
	<b>Residential</b>	<b>Lodging</b>	<b>Office</b>	<b>Retail</b>
Residential	1.0	1.1	1.4	1.2
Lodging	1.1	1.0	1.7	1.3
Office	1.4	1.7	1.0	1.2

<b>Table 3835: Shared Parking Factors</b>				
<b>Use Sharing Parking</b>	<b>Use Sharing Parking</b>			
	<b>Residential</b>	<b>Lodging</b>	<b>Office</b>	<b>Retail</b>
Retail	1.2	1.3	1.2	1.0

**Sec. 24-3836.** *Parking Standards: Parking Location and Access*

- A. Parking shall be located in accordance with Sec. 24-3824 Frontage Standards.
- B. All parking shall be accessed from rear alleys where they exist (see diagram) or from side streets if the lot is located on a corner. If no rear alley or side street exists, access shall be shared with neighboring properties to the extent practical.
- C. When access to rear parking must be directly from the primary street, driveways shall be located along side property lines (see Figure 3836: Parking Access Location) and designed such that pedestrians crossing on sidewalks always have the right-of-way.

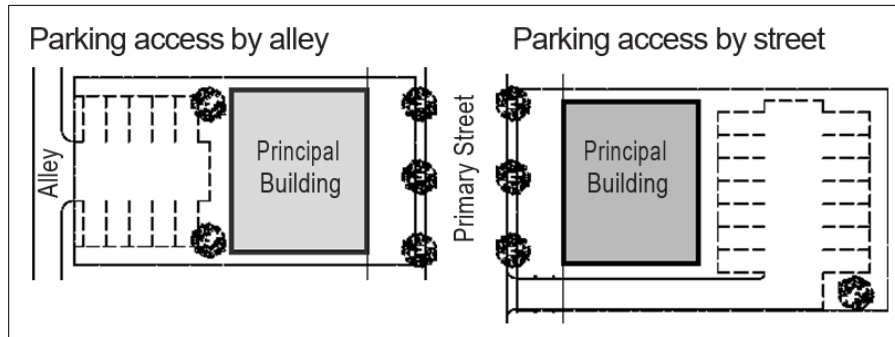


Figure 3836: Parking Access Location

**Sec. 24-3837.** *Standards for New Buildings: Primary Entrances*

The primary entrance of every building that provides access to a principal use shall face a street or a public space. A public space may include a central garden or courtyard that opens directly onto the primary street.

**Sec. 24-3838.** *Standards for New Buildings: Wide Buildings*

In order to soften the visual effect of wide buildings directly across the street from narrower buildings, the primary façade of buildings wider than 150 feet shall include at least one of the following:

- A. A break in the façade plane extending the full height of the primary façade or a wall projection or molding the full height of the primary façade; or
- B. A change in the roof line consisting of a variation in building height or a variation in roof form to divide the building into different components.

**Sec. 24-3839.** *Standards for New Buildings: Façade Transparency*

All building façades which face onto a street or public space shall comply with the minimum transparency standards in Table 3839A: Minimum Façade Transparency. An example of appropriate façade

transparency is shown in Figure 3839B: Illustration of Façade Transparency Requirements. For retail spaces, posters or signs are permitted provided that they are temporary and do not significantly reduce the ability to see into a space in operation.

<b>Table 3839A: Minimum Façade Transparency</b>	
Building Story and Use	Minimum Building Façade Transparency [1]
Ground story, retail use	60 percent
Ground story, uses other than retail	30 percent
Upper story, all uses	30 percent

**NOTES:**  
 [1] Measure of the amount of transparent window area or door openings in the façade of a building relative to the overall surface area of the façade. The percentage of transparency per story shall be calculated within the area between finished floors and shall be a total percentage of doors and windows along that portion of the façade.

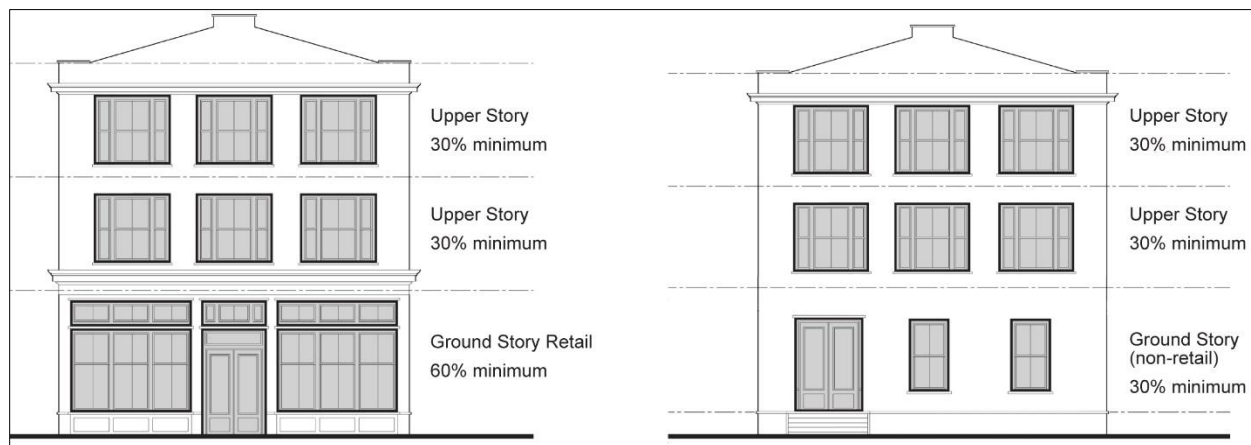


Figure 3839B: Illustration of Façade Transparency Requirements

**Sec. 24-3840.** *Standards for New Buildings: Frontage Elements: Purpose and General Requirements*

- A. Frontage elements are semi-private elements of a building typically located in the area between the primary façade and the lot line. Frontage elements create the transition between the public realm and private buildings and establish the physical and functional relationship between the street and the private lot. This section identifies common frontage elements and sets out standards that apply to those elements. Frontage elements are allowed, but not required. However, if a frontage element is used, it must conform to the standards in this section.
- B. Frontage elements shall be located between the build-to zone or setback line and the right-of-way.
- C. Enclosed useable space shall be permitted above an arcade or gallery forward of the build to zone or setback.

- D. Arcades, Galleries, and Porches may wrap around the side of the building to face side streets on corner lots.
- E. Frontage elements on commercial buildings shall maintain a clear envelope of space with 10' head clearance and 10' between the building and any other element that reaches the ground.

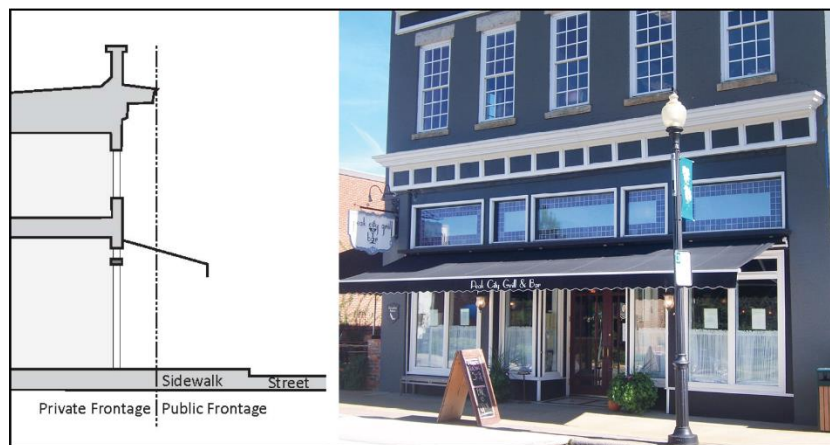
**Sec. 24-3841.** *Standards for New Buildings: Frontage Elements: Encroachments within Right-of-Way:*

- A. Arcades, Galleries, Balconies, and Awnings may encroach into the right-of-way only with the agreement of the agency controlling the right-of-way (VDOT, DPW), but may not encroach within a DPU easement. The agreement shall establish the property owner's sole responsibility for repairing any damage that may result from public maintenance or improvements. Enclosed useable space above an arcade or gallery forward of the right-of-way must be approved as part of the agreement.
- B. Frontage elements shall not extend closer than three feet from the curb line.

**Sec. 24-3842.** *Standards for New Buildings: Frontage Elements: Commercial or Shopfront Frontage Elements*

A. Shopfront

A shopfront is a frontage element in which the building façade is aligned close to the property line with a high percentage of glazing conventional for shop displays and dining. The building entrance is generally at sidewalk grade. It is often used with arcades, colonnades, galleries, awnings, canopies, or other shade devices. See Figure 3842A-1: Shopfront Illustrations.



*Figure 3842A-1: Shopfront Illustrations*

1. Shopfronts shall have a cornice or expression line above, between the first and second story (see Figure 3842A-2: Shopfront Transparency and Expression Line). Expression lines shall either be moldings extending a minimum of two inches outward from the primary façade, or jogs in the surface plane of the building wall greater than two inches.



*Figure 3842A-2: Shopfront Transparency and Expression Line*

- 2.** The entrances to all shopfronts shall be covered, either by an awning, canopy, second floor balcony, arcade, colonnade, or gallery, or by being inset into the main body of the building.
- 3.** Entrances for public access into commercial areas shall be provided at intervals no greater than 50 feet.
- 4.** Shopfront doors shall contain at least 50 percent transparent glass.
- 5.** The top of all shopfront windowsills shall be between one and three feet above the adjacent sidewalk. Shopfront windows shall extend up from the sill at least eight feet above the adjacent sidewalk (see Figure 3842A-2: Shopfront Transparency and Expression Line).
- 6.** The ground floor of a shopfront shall have untinted transparent storefront windows or doors covering no less than 60 percent of the wall area. See Sec. 24-3839: Façade Transparency.
- 7.** Shopfront windows shall not be made opaque by window treatments (excepting operable sunscreen devices within the conditioned space). Reflective and frosted glass is prohibited on shopfronts. Low emissivity glass with high visual light transmittance is permitted.
- 8.** Storefronts must remain unshuttered at night to provide views of display spaces and are encouraged to remain lit from within until 10:00 PM to provide security to pedestrians.

#### B. Arcade, Colonnade, or Gallery

An arcade, colonnade, or gallery is a frontage element where the building façade is aligned close to the property line with an attached cantilevered shed or a colonnade overlapping the sidewalk. See Figure 3842B: Arcade/Colonnade/Gallery Illustrations.

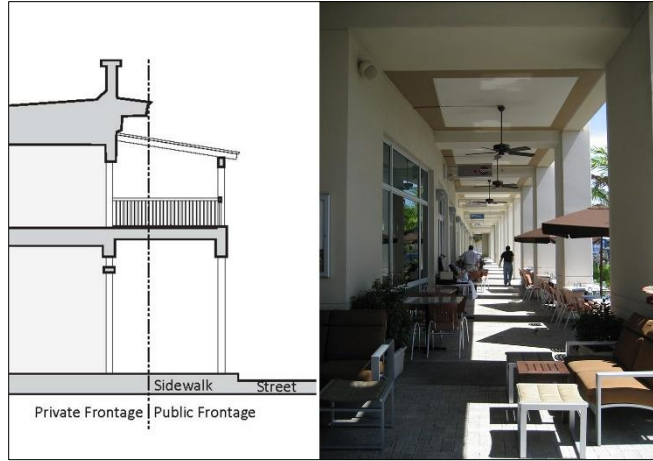


Figure 3842B: Arcade/Colonnade/Gallery Illustrations

1. The sidewalk may be a combination of public and private sidewalk.
2. The Arcade/Gallery shall extend a minimum of 75 percent of the building frontage.
3. This frontage element type is conventional for retail uses.

#### C. Forecourt

A forecourt is a frontage element where the primary portion of the building's main Facade is at the Build-to Zone while a small percentage is set back, creating a court space. This space can be used as an apartment entry court, garden space, or for restaurant outdoor dining. A forecourt should be used in conjunction with other frontage element types. See Figure 3842C: Forecourt Illustrations.



Figure 3842C: Forecourt Illustrations

#### D. Terrace

A terrace is a frontage element where the building façade is set back from the property with a relatively level paved or planted area adjoining a building and connecting it to the sidewalk. It takes up more space than a forecourt and is often elevated. A Terrace may be used in conjunction with other frontage types. This frontage element tends to buffer residential uses from urban sidewalks and removes the private yard from public encroachment. Terraces are suitable for use as, or conversion to, outdoor cafes. See Figure 3842D: Terrace Illustrations.



Figure 3842D: Terrace Illustrations

### E. Awning

1. The minimum depth for awnings over ground-story doors or windows is five feet.
2. Awnings must extend over at least 25 percent of the width of the primary façade.

## **Sec. 24-3843.** *Standards for New Buildings: Frontage Elements: Residential Frontage Elements*

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### A. Stoop

A stoop is a frontage element in which the residential building facade is aligned close to the property line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair with a landing that is shallower in depth than a porch. A stoop is recommended for ground-floor residential use. See Figure 3843A: Stoop Illustrations.

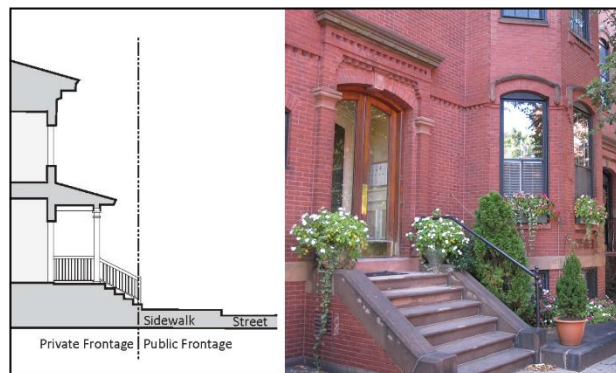


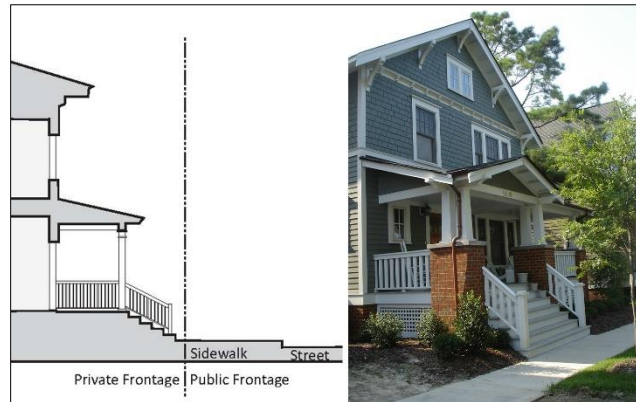
Figure 3843A: Stoop Illustrations

1. Minimum finished stoop floor height shall be at or up to eight inches below the first interior finished floor level, not to exceed 42 inches above the finished grade of the sidewalk.
2. Stoop stairs may run to the front or to the side.
3. Stoops shall be covered, either with a roof, or area inset into the main body of the building.
4. Partial walls and railings on stoops shall be no higher than 42 inches.



## B. Porch

A porch is a frontage element where the building facade is set back from the property line, the ground floor is elevated, and stairs lead up to a landing that is large enough for people to gather and can accommodate patio furniture. A fence or low knee wall may be located at the property line to maintain street spatial definition. See Figure 3843B: Porch Illustrations.



*Figure 3843B: Porch Illustrations*

1. Minimum porch depth is eight feet.
2. Minimum finished porch floor height shall be at or up to eight inches below the first interior finished floor level, not to exceed 42 inches above the finished grade of the sidewalk.
3. Front porches may occur forward of the build-to zone or setback but shall not extend into the right-of-way or any easement.
4. Side porches may extend past the side setback requirements, but not into any easement.
5. Front and side porches may be screened; however, if screened, columns, railings, etc., must be located on the outside of the screen facing a street or public space.

## C. Balcony

1. Minimum depth is six feet.
2. Balconies must extend over at least 25 percent of the width of the primary façade. See Figure 3843C: Balcony Example.



*Figure 3843C: Balcony Example*

3. Balconies may have roofs but must be open toward the primary street.
4. Open balconies are permitted over galleries above the sidewalk level.

#### **Sec. 24-3844.** *Specialty Buildings: Purpose*

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The purpose of the standards for specialty buildings is to ensure any auto-oriented development permitted in the FBA-O District does not detract from the walkability of the district. There are three types of specialty buildings:

- A. Liner Buildings;
- B. Automobile Filling Stations; and
- C. Drive-throughs

#### **Sec. 24-3845.** *Specialty Buildings: Liner Buildings*

---

- A. Parking structures and parking decks shall be separated from adjacent streets (but not alleys) by liner buildings, to the maximum extent practicable.
- B. Liner buildings must be at least two stories in height with no less than 15 feet in depth (see Figure 3845: Liner Building Configuration).

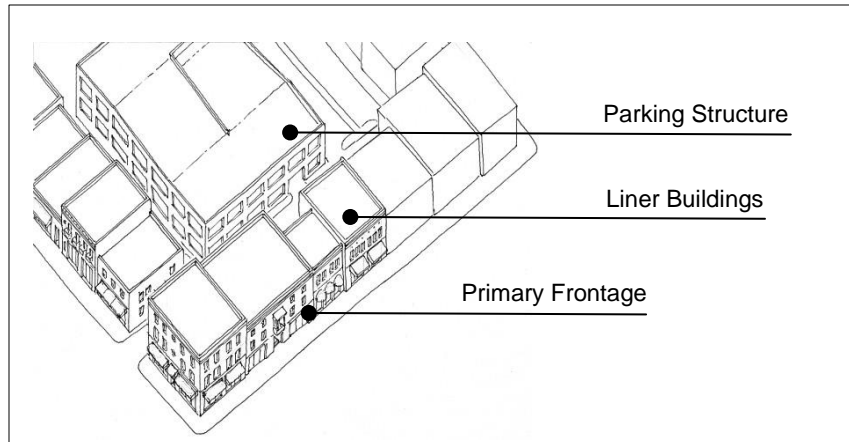


Figure 3845: Liner Building Configuration

- C. Liner buildings may be detached from or attached to the primary building (see Figure 3845: Liner Building Configuration).
- D. When access to a parking structure or deck cannot be accommodated on an alley, the required liner building may have a break of up to 30 feet to allow vehicular access on the lowest hierarchy street the parcel adjoins.
- E. Liner buildings may be occupied for any use permitted and as regulated in the district, except for parking.
- F. Liner buildings must comply with the primary façade transparency standards in Sec. 24-3839, Standards for New Buildings: Façade Transparency.

**Sec. 24-3846. Specialty Buildings: Automobile Filling Stations**

- A. A ground-floor shopfront is required. The shopfront shall face the primary street and if it is located on a corner lot shall be located at the build-to line at the corner of the lot.
- B. All pumps, parking, and drive-through areas must be located behind the building.
- C. An example of an appropriate automobile filling station configuration is shown in Figure 3846: Automobile Filling Station Configuration.

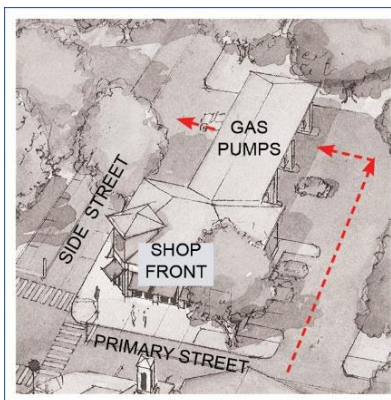


Figure 3846: Automobile Filling Station Configuration

**Sec. 24-3847.** *Specialty Buildings: Drive-throughs*

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- A. A ground-floor shopfront must face the primary street.
- B. All parking shall be located in the rear and accessed from a rear alley when present.
- C. Drive-through windows shall be located to the side or rear of the building.
- D. Drive-through standards may be altered through approval of a provisional use permit in accordance with Sec. 24-2.3( e), Provisional Use Permit.
- E. An example configuration of an appropriate building with a drive-through is shown in Figure 3847: Drive-through Building Configuration.

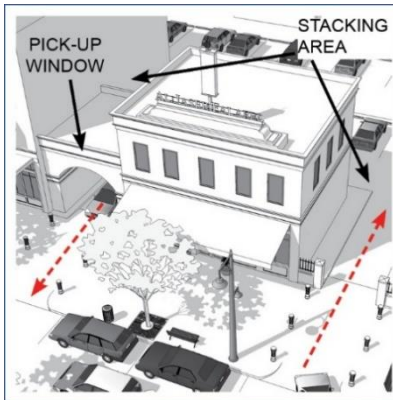


Figure 3847: Drive-through Building Configuration

**Sec. 24-3848.** *Signage*

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Signage in the FBA-O District shall comply with the following standards, in addition to the standards in Article 5. Division 8, Signage.

- A. Signs must be designed and scaled for use by pedestrians, in accordance with the following standards:
  - 1. Signs mounted flat against a building façade shall be limited to a band having a maximum height of three feet on any building façade. The maximum gross area of all such signs mounted on a building façade shall not exceed ten percent of the façade area.
  - 2. Signs mounted on and perpendicular to a building façade shall maintain a minimum vertical clearance of eight feet above sidewalks and shall not extend closer than two feet from the curb line. The maximum area of any such sign shall be nine square feet.
- B. Signage must be consistent within the subdistrict in theme and placement, and coordinated with other streetscape furniture (e.g., light posts) to reduce visual clutter in the public realm.
- C. Signage shall be coordinated with streetscape plantings to allow required plantings without reducing the visibility of signage and businesses.
- D. Detached signs are prohibited, except as provided in Sec. 24-3.7(j)10a.3.
- E. Banner signs shall comply with the following standards:

1. The maximum size of the banner shall be four feet in height and 40 feet in length.
  2. Banner signs shall be mounted only on light poles or other street furniture designed for the purpose of hanging banners. In addition, temporary banner signs may be hung over a roadway within a public right-of-way having a width of 70 feet or less.
  3. Banner signs shall not be illuminated.
  4. Banner signs over a public right-of-way shall be approved by the authority that maintain the road (either VDOT or DPW).
- F. Signs that are illuminated shall be externally illuminated from the front. Back lighting is permitted as an exception only for individual letters or numbers (panelized back lighting is prohibited). Signage within a storefront may be neon lit.

### **Sec. 24-3849.** *Exterior Lighting*

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#### A. Parking Lot Lighting

1. Light fixtures located within the interior of a parking lot shall not exceed 30 feet in height.
2. Light fixtures located along the perimeter of a parking area within 50 feet of a property line shall not exceed 15 feet.

#### B. Pedestrian Walkway Lighting

1. Light fixtures located along pedestrian walkways adjacent to parking lots shall not exceed 15 feet in height.
2. Light fixtures located along internal pedestrian walkways or paths not adjacent to a parking area or a street shall not exceed 10 feet in height.

#### C. Building & Security Lighting

1. All exterior building or security lighting must be full cutoff, shielded, and/or angled downward to focus the light only on the intended doorway or walkway.
2. Security lighting is encouraged to be provided with regular pedestrian light fixtures where visible from the street or public way to match other fixtures used on site.
3. Building-mounted architectural "accent lights" are permitted when downward facing only.
4. Storefront business owners are encouraged to assist with lighting the sidewalk and to accent their business location by leaving display window and interior lighting on at night. Lighting shall be designed in such a way as to prevent the direct view of the light source to neighboring residential areas.

*Secs. 24-3850 through 24-3858 Reserved.*

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**Sec. 24-3859.** *FBA-O Subdistricts*

The FBA-O District consists of the subdistricts identified in Table 3859: FBA-O Subdistricts. This section sets out the intent of each subdistrict and establishes standards specific to each subdistrict that apply in addition to the other FBA-O District standards.

<b>Table 3859: FBA-O Subdistricts</b>
Brookfield Area Subdistrict
Parham/Broad Area Subdistrict
Williamsburg Road Area Subdistrict
Virginia Center Commons Subdistrict

**Sec. 24-3860.** *Brookfield Area Subdistrict: Boundary*

The boundary of the Brookfield Area Subdistrict is identified on the Zoning Districts Map.

**Sec. 24-3861.** *Brookfield Area Subdistrict: Intent*

The Brookfield Area is currently an office park with a single land use and large areas of surface parking. The intent of this section is to identify a set of design principles and establish regulations that support development in the area that results in a vibrant and lively complete neighborhood with a network of streets, a mix of uses, and public civic space.

**Sec. 24-3862.** *Brookfield Area Subdistrict: Illustrative Plan*

Figure 3862: Brookfield Area Illustrative Plan, depicts an ideal buildout of the Brookfield Area and illustrates design principles encompassed within the FBA-O District standards that help to create a more appealing public realm (streets, civic spaces) and a more pedestrian friendly environment (places to walk), within a mixed-use neighborhood. The numbered labels in Figure 3862 correspond to the design principles in subsections A through G below.





Figure 3862: Brookfield Area Illustrative Plan




- A. The open side of a parking structure along a street edge is not appealing to pedestrians or conducive to a lively environment. A liner building should be placed between the parking structure and street or civic space. Retail, shops, offices, or even residential uses can occupy the liner building to activate the public realm along the sidewalk. Access to parking structures should be from secondary streets or alleys to minimize conflicts between pedestrians and cars.
- B. Buildings placed along the street edge of a block create a mid-block area that is less visible from the public view but is a convenient location for surface parking, structured parking, or service areas. Alleys for vehicular circulation are used to easily access these areas.
- C. Existing trees with a diameter at breast height (DBH) that is more than 10 inches, forest areas, and native species with high ecological value should be preserved.
- D. New buildings should include a mix of uses and building types. New buildings will be placed in accordance with the development areas shown on the regulating and street hierarchy plan.
- E. Existing residential areas will remain residential with a transition to the more urban areas.
- F. Smaller block sizes promote walkability. New streets are introduced to break down the existing large blocks and create connections to adjacent streets.
- G. New civic spaces provide places for the community to gather, enjoy outdoor activities, and provide a focus for the subdistrict.



**Sec. 24-3863.** *Brookfield Area Subdistrict: Potential Development Phasing*

Future development in the Brookfield Area may be phased to allow incremental investment. Table 3863: Potential Phases of Brookfield Area Development, illustrates a possible sequence for phased development.

**Table 3863: Potential Phases of Brookfield Area Development**

Existing Conditions	Initial steps
 <p data-bbox="444 835 594 865">Initial Steps</p>	<p data-bbox="831 474 1414 848">Initial investments can happen along West Broad Street. This location allows maximum visibility for new businesses and creates a new gateway for the area. Building frontages on West Broad Street catch people’s attention and signal a different kind of development. An access lane parallel to West Broad Street allows vehicular access and provides parallel on-street parking. The new access lane should have continuous sidewalks, street trees, street furnishings, and lighting to create an inviting experience walking from one building to the next.</p>
 <p data-bbox="435 1243 571 1272">Phase Two</p>	<p data-bbox="831 877 1386 1293"><b>Phase two</b> A second phase of development could occur along two new streets, Street A which lies to the north of Genworth Financial and Street B which aligns with an existing drive between Brookfield Commons and the 7-story office tower. The streets have sidewalks and street furnishings added to the existing street trees, and pedestrian-scaled lighting. Buildings are placed along the two street edges to create a dynamic pedestrian realm. New development along these streets vary to create different characters based on the underlying development area.</p> <ul data-bbox="870 1314 1414 1617" style="list-style-type: none"><li>• Buildings along Street A are larger and taller in size. Buildings such as an 8- to 12-story corporate headquarters building, and 2- to 4-story mixed-use apartment buildings create an edge between development and the interstate.</li><li>• Street B should be a commercial main street. Buildings fronting Street B would have a smaller scale along with more shops and retail.</li></ul>
	

**Table 3863: Potential Phases of Brookfield Area Development**

**Phase Three**

Phase three shows future mixed-use apartment development along Dickens Road. The buildings sizes will be less than four stories, to allow a transition to the existing single-family houses adjacent to the site.



**Sec. 24-3864. Brookfield Area Subdistrict: Regulating and Street Hierarchy Plan**

The Regulating and Street Hierarchy Plan for the Brookfield Area Subdistrict is set out in Figure 3864: Brookfield Area Regulating and Street Hierarchy Plan.

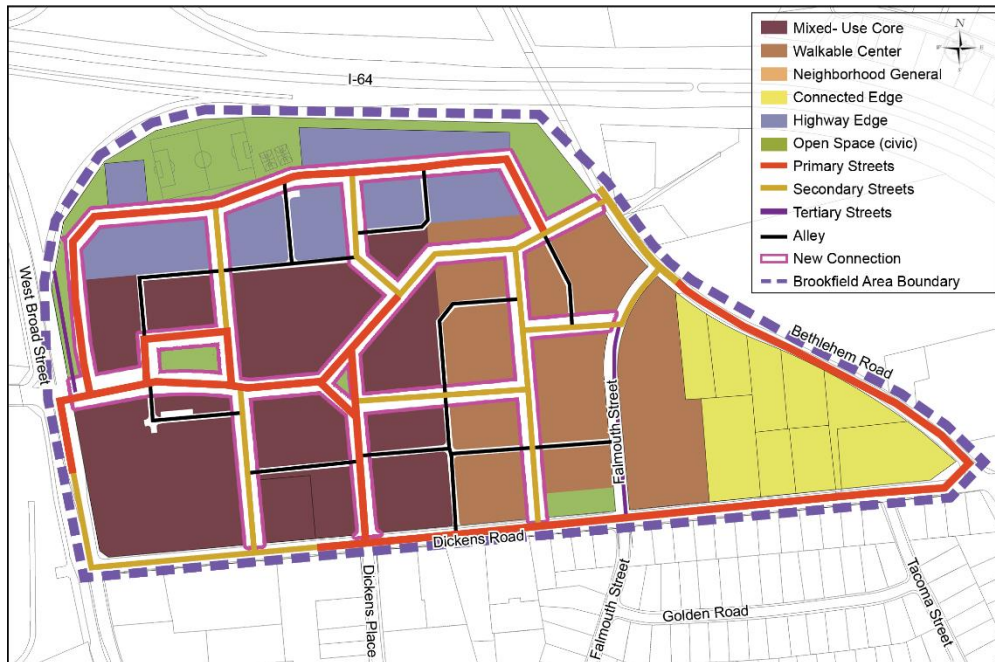


Figure 3864: Brookfield Area Regulating and Street Hierarchy Plan

**Sec. 24-3865.** *Brookfield Area Subdistrict: Additional Brookfield Area Standards*

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Monument Signs are permitted along West Broad Street.

**Sec. 24-3866.** *Parham/Broad Area Subdistrict: Boundary*

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The boundary of the Parham/Broad Area Subdistrict is identified on the Zoning Districts Map.

**Sec. 24-3867.** *Parham/Broad Area Subdistrict: Intent*

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The Parham/Broad Area is located around the intersection of West Broad Street and East Parham Road. The area consists mainly of shopping centers and car dealerships with large parking areas. The intent of this section is to identify a set of design principles and establish regulations that support more walkable development with storefronts facing streets such as West Broad Street transitioning to residential uses towards the edges.

**Sec. 24-3868.** *Parham/Broad Area Subdistrict: Illustrative Plan*

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Figure 3868: Parham/Broad Area Illustrative Plan, depicts an ideal buildout of the Parham/Broad Area and illustrates design principles encompassed within the FBA-O District standards that help to create a more appealing public realm, with the different parcels developed in a way that benefits both the property owners and residents. The numbered labels in Figure 3868 correspond to the design principles in subsections A through L below.



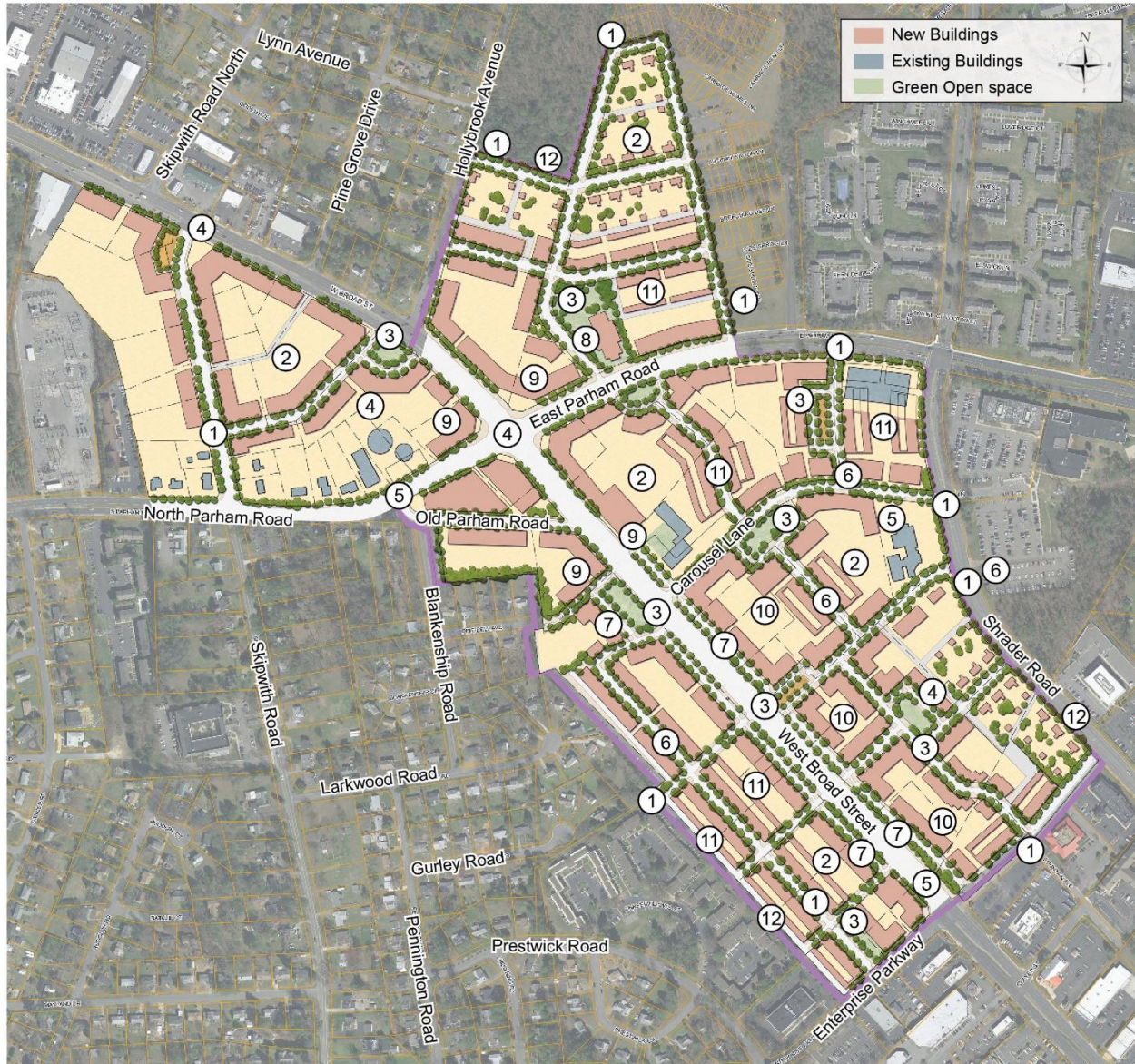


Figure 3868: Parham/Broad Area Illustrative Plan

- A. Establish a network of connected streets.
- B. Create a series of blocks that have a perimeter with buildings placed close to the street. Buildings should line the block so that it is possible to keep the middle of the block open for surface parking, or structured parking. Access to the interior of the block should be through lanes and alleys.
- C. Establish a variety of paved plazas and green spaces to act as gathering places for the public.
- D. Create terminated vistas and special views using the street network, topographical features, and civic spaces.
- E. Plant trees along streets to provide shade, clean rainwater, and improve the look and feel of the area.



- F. Encourage parallel parking on existing and new streets.
- G. Incorporate the use of a “clockwise access lane” along West Broad Street so people can access commercial uses by turning in front of them and accessing either on-street parking or the rear parking areas.
- H. Add civic functions to capitalize on investment and create a more complete neighborhood.
- I. Allow for a vertical mix of uses and place these buildings along West Broad Street and East Parham Road.
- J. Allow apartment buildings by right.
- K. Allow row houses by right.
- L. Transition to nearby residential neighborhoods using row houses and single-family detached residences.

**Sec. 24-3869.** *Parham/Broad Area Subdistrict: Potential Phasing of Development*

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As development occurs in the Parham/Broad Area subdistrict, it will likely occur in phases or on a parcel by parcel basis. Table 3869: Potential Progression of Parham/Broad Area Development, illustrates a possible progression for development in the subdistrict. Each phase should complete the strategies as completely as possible so that each new project builds toward the whole in creating a transformed neighborhood.

**Table 3869: Potential Progression of Parham/Broad Area Development**



**Existing Conditions**

Existing Conditions in the Parham/Broad Area are characterized with large parking areas to service shopping centers and used car dealerships.



**Build on Underutilized Parking**

Initial development could take place in unused parking lots, keeping existing buildings that are currently generating income for landowners. This image illustrates how development follows the strategies identified in the Illustrative Plan. A clockwise access lane is created along with a long green public space. Mixed-use buildings are placed close to the lane with parallel parking, street trees, and sidewalks.

**Table 3869: Potential Progression of Parham/Broad Area Development**



**Redevelopment of Built Areas**

As undeveloped areas of parking lots become built-out, property owners may then rebuild their parcels. Now, new streets and blocks begin to take shape and public spaces are being created. Buildings are built close to the street and leave the center of the block open for parking and back of house functions.



**Expanding Redevelopment**

Redevelopment and investment will encourage even more investment in the area. This development will need to respond to the residential areas adjacent to the Parham/Broad Area. This can be done with residential housing types that may not be found in the area but provide additional housing options while transitioning from the mixed-use core to the surrounding single-family residential areas such as row houses.

**Complete Neighborhood**

Once the Parham/Broad Area has been built out, a variety of building types will have been constructed with a vertical mix of uses, civic structures, greens, and plazas. A connected network of streets and blocks makes it possible for residents to walk to these new gathering spaces and shop and dine at the locations in the area that has been transformed into a complete neighborhood.





**Sec. 24-3870.** *Parham/Broad Area Subdistrict: Regulating and Street Hierarchy Plan*

The Regulating and Street Hierarchy Plan for the Parham/Broad Area subdistrict is set out in Figure 3870: Parham/Broad Area Regulating and Street Hierarchy Plan.

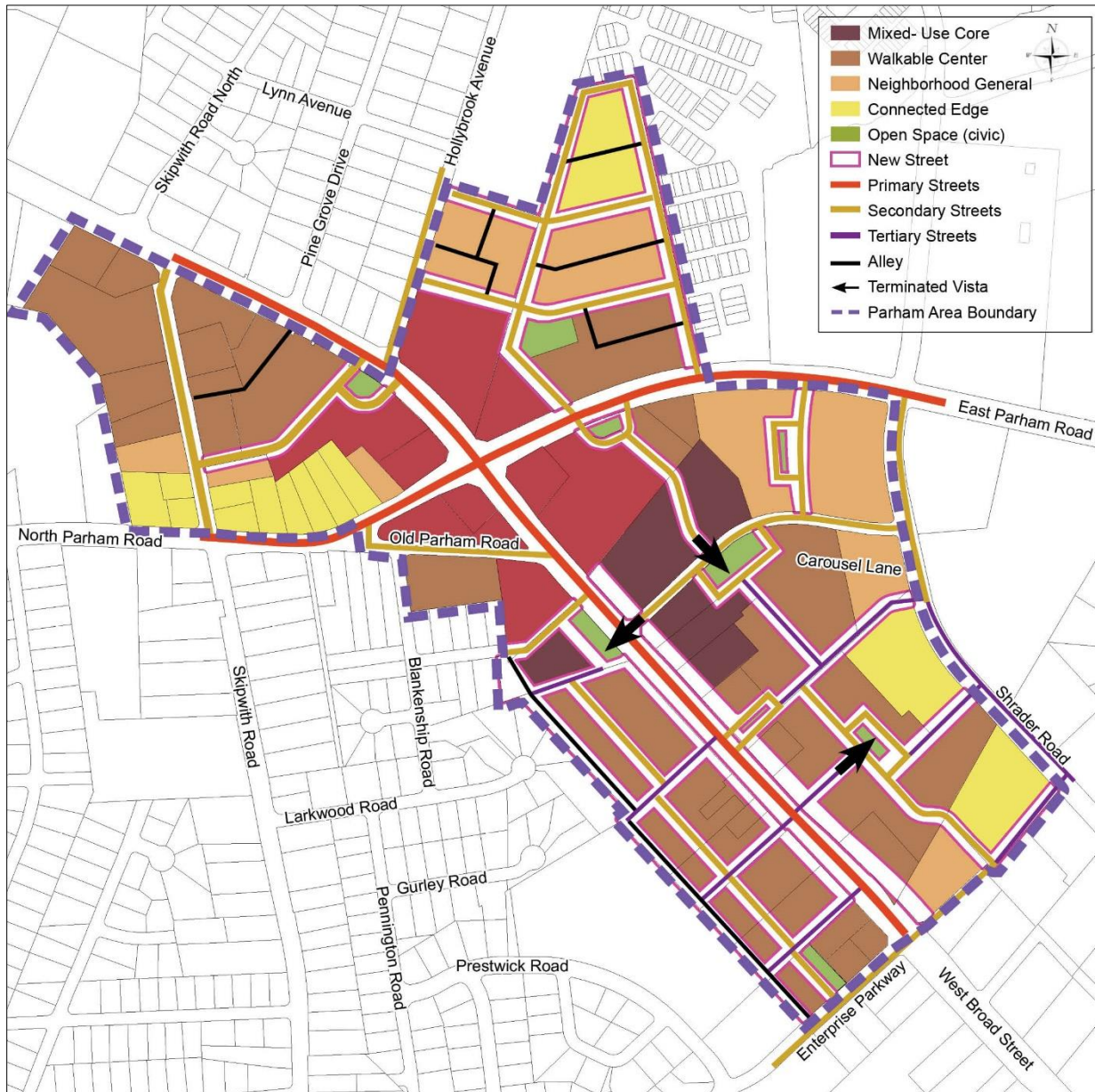


Figure 3870: Parham/Broad Area Regulating and Street Hierarchy Plan



**Sec. 24-3871.** *Parham/Broad Area Subdistrict: Additional Parham/Broad Area Standards*

In addition to the allowed uses established in Article 4: Use Regulations, Auto Services and Auto Sales are permitted.

**Sec. 24-3872.** *Williamsburg Road Area Subdistrict: Boundary*

The boundary of the Williamsburg Road Area Subdistrict is identified on the Zoning Districts Map.

**Sec. 24-3873.** *Williamsburg Road Area Subdistrict: Intent*

Williamsburg Road is a thoroughfare immediately adjacent to the Richmond International Airport. Hotels lining the street that historically attracted travelers arriving by plane have declined, and strip mall shopping is prevalent. The high visibility of this area from the airport provides an opportunity to draw visitors to the area while also providing amenities and services to the local community. The intent of this section is to identify a set of design principles and establish regulations that support a variety of housing options and the redevelopment of the shopping plaza at Williamsburg Road and Laburnum Avenue into a neighborhood center, connected by Williamsburg Road to a new center at the edge of the airport property.

**Sec. 24-3874.** *Williamsburg Road Area Subdistrict: Illustrative Plan for Subdistrict*

Figure 3874: Williamsburg Road Area Illustrative Plan, depicts an ideal buildout of the Williamsburg Road Area and illustrates design principles encompassed within the FBA-O District standards, showing how the different parcels can be developed in a way that benefits both the property owners and residents. The numbered labels in Figure 3874 correspond to the design principles in subsections A through F below.



Figure 3874: Williamsburg Road Area Illustrative Plan

A. Establish a network of connected streets.

- B. Create a series of blocks that have a perimeter with buildings placed close to the street. Buildings should line the block so that it is possible to keep the middle of the block open for surface or structured parking. Access to the interior of the block should be through lanes and alleys.
- C. A variety of paved plazas and green spaces become gathering spaces for the community.
- D. Uses can be mixed within the same building or adjacent to one another.
- E. Apartment buildings along the corridor provide additional housing while providing a transition to the adjacent neighborhood.
- F. Row houses provide additional housing options.

**Sec. 24-3875.** *Williamsburg Road Area Subdistrict: Focus Area 1: Airport-Owned Land*

A portion of flat land near the Richmond International Airport is a good location for a neighborhood development with a mix of uses including new hotels and public spaces. The area along Williamsburg Road to the east of Lewis Road, referred to as Focus Area 1, has the potential to develop into a small walkable center with connections to the airport.

A. Illustrative Plan for Focus Area 1

Figure 3875A: Illustrative Plan for Focus Area 1, illustrates the potential of Focus Area 1 to develop into a small walkable center with connections to the airport. The numbered labels in Figure 3875A correspond to the design principles in subsections 1 through 9 below.

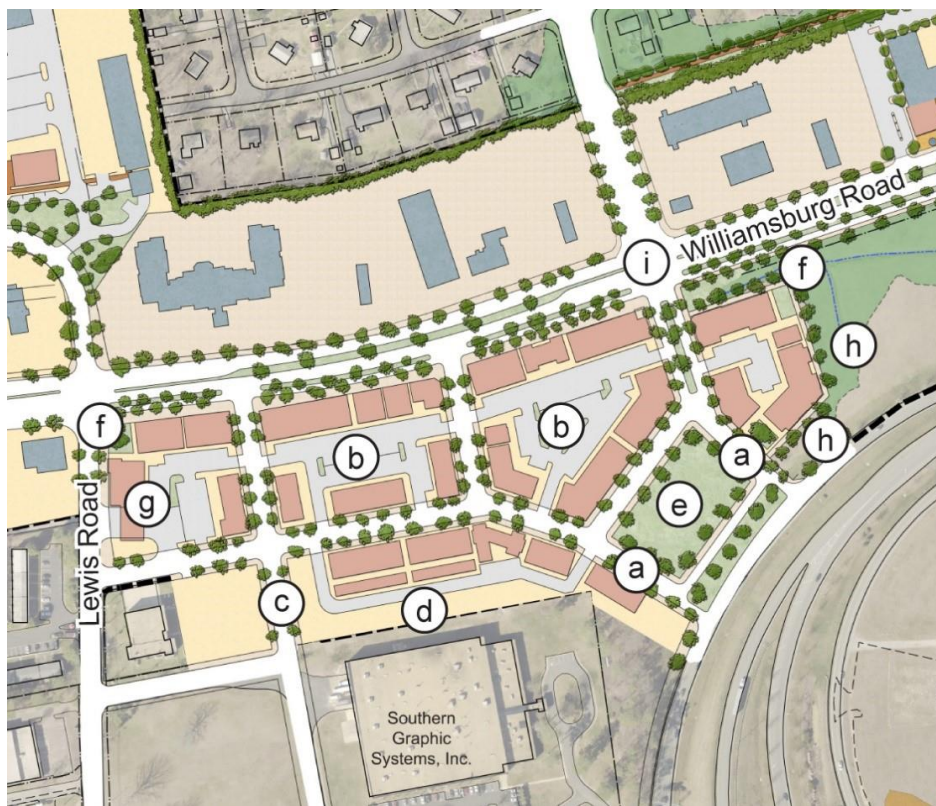


Figure 3875A: Illustrative Plan for Focus Area 1



1. Establish a network of streets that connects Williamsburg Road to Airport Road.
2. Parking is located behind buildings that front the street.
3. A new street connection to an existing private road provides additional connections to the neighboring development.
4. The back of parcels are adjacent to the backs and sides of neighboring parcels to create a transition between building types and uses.
5. A public green is established to create a gathering space to watch airplanes take off and land.
6. Small parks create a variety of public spaces.
7. A new fire station can serve the surrounding area.
8. New buildings look out over green spaces.
9. A new intersection is created to connect with Sanburne Parkway.

B. Visualization of Potential Focus Area 1 Development

Table 3875B: Renderings of Potential Focus Area 1 Development, includes renderings, together with descriptive text, that were developed to visualize how the key components of the airport-owned land could be developed.

**Table 3875B: Renderings of Potential Focus Area 1 Development**



**Looking Southwest Across Williamsburg Road**

A network of streets connects Williamsburg Road to Airport Road. A retired airplane is parked in a public green to create a connection to the airport with a smaller park closer to Williamsburg Road. Parking is located behind buildings that are built close to the road.



**Looking Northwest Across Williamsburg Road**

A green public space acts as a gathering area for the community with development close to the street, tree-lined streets, parallel parking, a vertical mix of uses, and a variety of housing options.

**Table 3875B: Renderings of Potential Focus Area 1 Development**



**Looking Southeast Over Williamsburg Road**

Buildings are located close to the road, with a corner green at the intersection with Lewis Road. A two-way cycle track is incorporated into the public right-of-way. A vertical mix of uses makes Williamsburg Road lively, while apartments and row houses transition to residential neighborhoods.

**Sec. 24-3876.** *Williamsburg Road Area Subdistrict: Focus Area 2: Redevelopment of Older Parcels*

The area along Williamsburg Road currently has properties that hold potential to activate the road and improve the quality of life for residents. The development of each parcel should enable and encourage good development in neighboring parcels. This section includes an enlarged illustrative plan and renderings that depict how a single parcel might develop over time.

A. Illustrative Plan for Example Parcel

Figure 3876A: Illustrative Plan for Focus Area 2 Example Parcel, illustrates the potential redevelopment of one parcel to show how individual parcels can redevelop and add to the overall intent of making the Williamsburg Road area more walkable. The numbered labels in Figure 3876A correspond to the key features described in subsections 1 through 9 below.



Figure 3876A: Illustrative Plan for Example Parcel

1. New streets inside the parcel establish blocks and circulation within the lot.
2. New streets may connect with neighbors in the future to create a larger network of streets.
3. Green spaces for gathering are created.
4. New landmarks are created using towers along key terminated vistas.
5. A mix of uses including retail and dining below residential and offices are built.
6. Playgrounds and community gardens are constructed in areas with natural surveillance.
7. The existing hotel has new rooms added closer to Williamsburg Road with a tower to increase visibility.
8. A biking and hiking trail between the existing parcel and neighbors to the north.
9. An alley is created along a side lot line to allow it to be used by new development on the adjacent parcel.

## B. Phased Development of Example Parcel

Redevelopment of existing parcels is likely to occur in phases. Table 3876B: Renderings of Phased Development of Example Parcel, includes renderings to illustrate how the phased development of an example parcel might progress, together with descriptive text.



**Table 3876B: Renderings of Phased Development of Example Parcel**



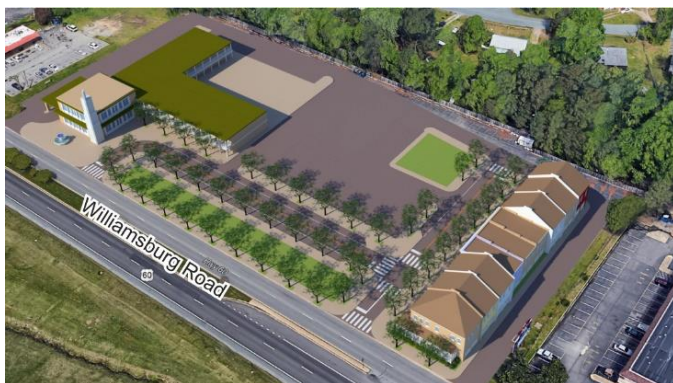
**Existing Conditions**

Existing development generally consists of large auto-oriented buildings surrounded by large parking lots.



**Downsize**

Initial stages of development will likely be driven by what the land or business owner sees as serving their investment goals. This illustration shows that a wing of the existing building is demolished, and an addition is built with a tower at the corner to draw attention and create a visual cue for visitors. This enables the existing structure to maintain operations during the redevelopment of the parcel.



**New Street Network and Uses**

New streets create a structure of lots and blocks, making circulation within the parcel possible and creating the opportunity for connections to neighboring parcels. This expands the street network, alleviating traffic from Williamsburg Road. A linear green space highlights the tower addition on the existing building while creating a green place to meet. New row houses provide housing opportunities. Any remaining demolition is completed.

**Table 3876B: Renderings of Phased Development of Example Parcel**



**Neighborhood Destinations**

After the final demolitions are completed and the internal street network is established, the last buildings can be constructed. Buildings with a vertical mix of uses, such as retail and dining with residences or offices above, can be built, along with additional apartments or row houses. Monuments may terminate views into the parcel from new streets. The illustration shows a small plaza with a monument at the end of a new street, which leads to a playground, community gardens, and a trail.

**Sec. 24-3877.** *Williamsburg Road Area Subdistrict: Focus Area 3: Neighborhood Center*

The area at the intersection of Laburnum Avenue and Williamsburg Road has potential to serve as a neighborhood center and destination. Figure 3877: Illustrative Plan for Focus Area 3, is an illustrative plan that shows how the shopping area at this intersection and areas adjacent to it may redevelop to create a walkable neighborhood center with a range of public spaces and destinations. The numbered labels in Figure 3877 correspond to the key features described in subsections 1 through 9 below.



Figure 3877: Illustrative Plan for Focus Area 3



1. New street connections are created while allowing existing buildings to remain and function for the community.
2. A neighborhood center is created around a civic green space.
3. A building is built at the end of a view into the neighborhood center creating a terminated vista. The building could be a civic building and have a use such as a school, meeting rooms, small library, or other civic function.
4. A connection to Bedford Street enables nearby residents access while avoiding the larger arterial roads.
5. The existing grocery store along with its associated parking remains.
6. Parking is located in mid-block locations.
7. New residential homes face the neighborhood to the north, creating transition to single-family residential neighborhood.
8. Backs of lots face the sides or backs of neighbors with vegetation to create a transition to the neighborhood to the south.
9. New buildings line Laburnum Avenue and Williamsburg Road to create an entrance into the neighborhood center.

**Sec. 24-3878.** *Williamsburg Road Area Subdistrict: Regulating and Street Hierarchy Plan*

The Regulating and Street Hierarchy Plan for the Williamsburg Road Area subdistrict is set out in Figure 3878: Williamsburg Road Area Regulating and Street Hierarchy Plan.

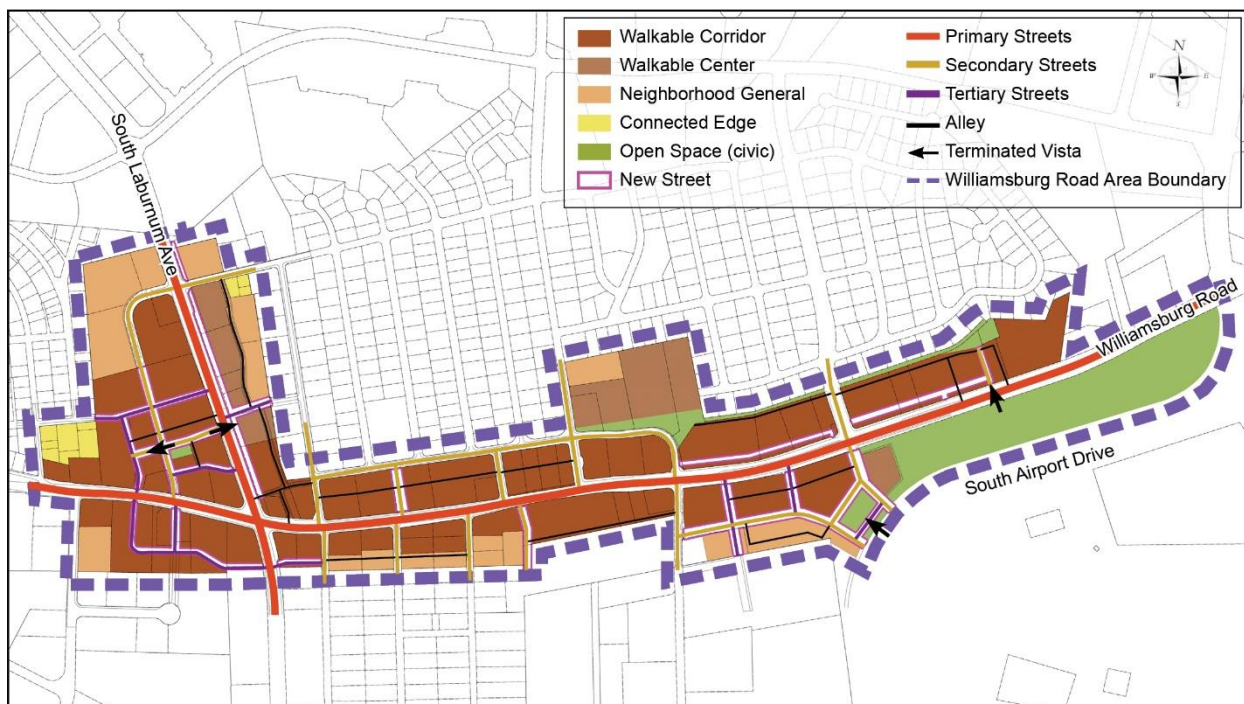


Figure 3878: Williamsburg Road Area Regulating and Street Hierarchy Plan

**Sec. 24-3879.** *Virginia Center Commons (VCC) Area Subdistrict: Boundary*

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The boundary of the Virginia Center Commons (VCC) Area Subdistrict is identified on the Zoning Districts Map.

**Sec. 24-3880.** *Virginia Center Commons (VCC) Area Subdistrict: Intent*

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The Virginia Center Commons (VCC) Area is located between US Route 1 and I-95, north of the I-95 and I-295 interchange. The area consists of the declining Virginia Center Commons Mall with several out parcels, hotels, a movie theater, and medical offices. This area is envisioned to become a mixed-use neighborhood housing a hotel, sports complex, retail and dining, along with a range of housing while incorporating existing uses as the market demands.

**Sec. 24-3881.** *Virginia Center Commons (VCC) Area Subdistrict: Illustrative Plan*

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The illustrative plan (see Figure 3881: VCC Area Illustrative Plan) depicts an ideal buildout of the VCC Area and illustrates how the different parcels can be developed in a way that benefits both the property owners and residents. The numbered labels in Figure 3881 correspond to the design principles in subsections A through N below.

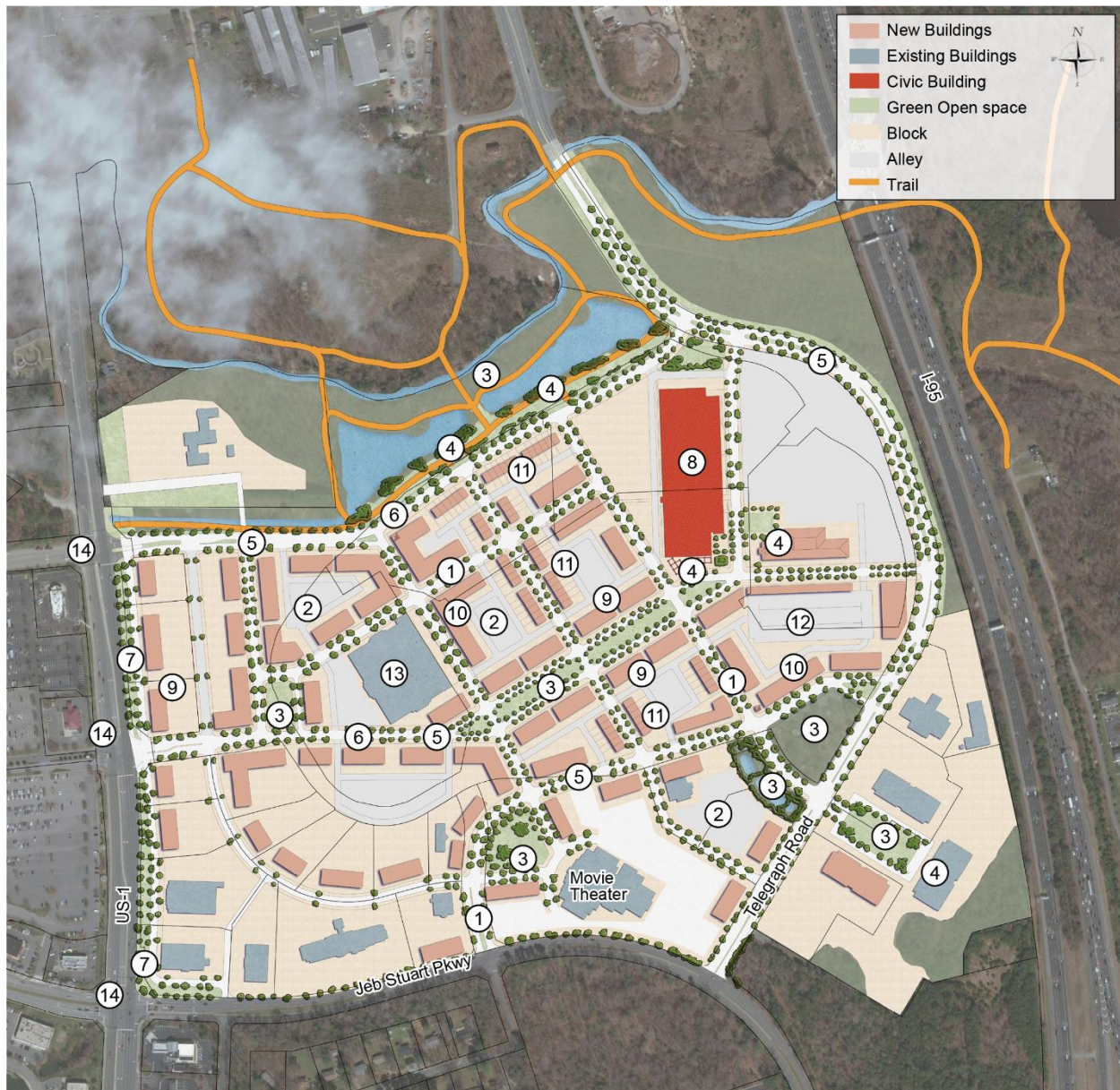


Figure 3881: VCC Area Illustrative Plan

- A. Establish a network of connected streets.
- B. Create a series of blocks that have a perimeter with buildings placed close to the street. Buildings should line the block so that it is possible to keep the middle of the block open for surface parking, or structured parking. Access to the interior of the block should be through lanes and alleys.
- C. Establish a variety of paved plazas and green spaces to act as gathering places for the public.
- D. Create terminated vistas and special views using the street network, topographical features, and civic spaces.



- E. Plant trees along streets to provide shade, clean rainwater, and improve the look and feel of the area.
- F. Encourage parallel parking on existing and new streets.
- G. Incorporate an access lane along US Route 1 so people can access commercial uses by turning in front of them and accessing either on-street parking or the rear parking areas. These access lanes would be a one-way clockwise access street, making them right-in and right-out from Route 1 to avoid conflicts resulting from left turns.
- H. Add civic functions such as the Sports Complex to capitalize on investment and create a more complete neighborhood.
- I. Allow for a vertical mix of uses and locate these along the “main street” of the area.
- J. Encourage the construction of apartment buildings.
- K. Encourage the construction of row houses.
- L. Construct parking structures as necessary and ensure that they have liner buildings.
- M. Incorporate existing structures from the mall as necessary to accommodate a flexible reuse and adaptation of space.
- N. Incorporate signalized crosswalks and bike lanes to encourage walking to the Virginia Center Commons.

**Sec. 24-3882.** *Virginia Center Commons (VCC) Area Subdistrict: Potential Development Phasing*

As development occurs in the VCC Area, it will likely occur in phases or on a parcel by parcel basis. Each phase should complete the strategies as completely as possible so that each new project builds towards the whole in creating a transformed neighborhood. Table 3882: Potential Progression of VCC Area Development, illustrates a possible progression for how different phases of development could occur:

**Table 3882: Potential Progression of VCC Area Development**



**Existing Conditions**

Existing conditions of the VCC Area are characterized by a large mall surrounded by large areas of parking.

**Table 3882: Potential Progression of VCC Area Development**



**Sports Complex & Hotel**

Plans are already under way to build a sports complex and hotel in the northeast portion of the site to capitalize on the sports tourism industry as well as provide recreational space for residents (Sec. 24-3883 below, illustrates alternative development). This can be achieved by removing some unused portions of the mall. New streets to access these uses will have street trees and parallel parking while larger parking lots are located behind the hotel. Green spaces introduce outdoor relaxation and recreation areas.



**Development of a "Main Street"**

As the mall consolidates, anchor tenant spaces may be reimaged with new uses while others could be removed. A "main street" can be created as a result, revising the course of the circular access road around the mall. This street could be wide with a long green park down the center leading to the sports complex and hotel. Buildings can start to line the street with a variety of uses as well as new residential types ranging from apartments to row houses.



**Expanding Redevelopment**

It is anticipated that redevelopment and investment will encourage investment in the area. A series of walkable blocks can be created with buildings facing the tree-lined streets, parking located mid-block, and the outdoor space improved with a range of green spaces including a green space in front of the movie theater. The brick structure of the existing food court could remain and create a gathering place. Large anchor tenant spaces may remain and be integrated within the block structure. A linear park along the Chickahominy River ties this potential development into the natural surrounding area with existing recreational trails.



## Table 3882: Potential Progression of VCC Area Development

### Complete Neighborhood

Eventually, most of the mall structure could be redeveloped, with the old food court structure remaining as a symbol of the site's history. Once the Virginia Center Commons Area has been built out, a variety of building types will have been constructed with a mix of uses, civic structures, greens, and plazas. A connected network of streets and blocks makes it possible for new residents that now live here to walk to these new gathering spaces while also shopping and dining within this complete neighborhood.



### **Sec. 24-3883.** *Virginia Center Commons (VCC) Area Subdistrict: Sports Complex Development Options*

Three options for the location of the sports complex and the hotel are shown for illustrative purposes in Table 3883: Sports Complex Development Options. These options show how the complex and hotel could be integrated within a walkable neighborhood, creating public spaces, and increasing the economic benefit of sports tourism for the area. A range of green spaces are included around the area for relaxation and recreation for residents and visitors alike. Parking garages and lots are located to accommodate cars while encouraging walking around the area. Key features within all three options include:

- A. Sports Complex;
- B. Possible hotel location;
- C. Plaza;
- D. Green space;
- E. Parking garage with liner building; and



F. Parking lot hidden by buildings.

**Table 3883: Sports Complex Development Options**



**Sport Complex Location – Option A**

In Option A, the sports complex creates a terminated vista at the end of a “main street” and faces south onto a plaza that could serve as a gathering space for large functions. A secondary entrance faces a parking lot. The complex is located near I-95 to maintain a visual presence. Several different locations are identified for the hotel – each is at the end of a street or public space to have high visibility while encouraging walking around the area.



**Sports Complex Location – Option B**

The sports complex faces east-west in Option B. The area in front of the complex is shown built-out, however it may start as a parking lot, eventually becoming a building site with a parking garage and a plaza. The rear of the sports complex faces an alley and a series of liner buildings. These buildings face a green space with views to the natural landscaping. The hotel is at the end of a linear green “main street” giving it high visibility within the area. Like the sports complex, the area behind the hotel may start as a parking lot and eventually become developed with additional buildings over time. A plaza with a monument to the north of the sports complex creates a gateway into the new neighborhood from the north.



**Sports Complex Location – Option C**

The sports complex in Option C is located at the center of the neighborhood with a plaza in front and a linear park leading to the recreational trails and creating a terminated vista from the trails and natural areas. The rear of the complex faces an alley and is hidden by liner buildings. Several locations are identified for the hotel – each being at the end of a street or near I-95 for high visibility.



**Sec. 24-3884.** *Virginia Center Commons (VCC) Area Subdistrict: VCC Food Court Structure Reuse*

Notable features of the Virginia Center Commons Mall are the brick structures found within the food court area. The shape is similar to a gothic cathedral with a tripartite division with a higher structure in the middle. Figure 3884: Illustration of Food Court Structure Reuse, illustrates how the brick structures could be saved and reinforced to create a unique gathering space within the neighborhood. The existing brick structure could remain and frame public space while serving a reminder of the history of the Virginia Center Commons. Shops, restaurants, food trucks, and other attractions could be located around the edges.



*Figure 3884: Illustration of Food Court Structure Reuse*

**Sec. 24-3885.** *Virginia Center Commons (VCC) Area Subdistrict: Multi-Use Trail*

As development progresses, a mixed-use trail could be installed in the linear green at the north of the VCC Area, connecting to the natural areas to the north. This would increase the options for relaxation and active recreation for the neighborhood and the community. The trail could connect to the existing trail network while also providing local amenities such as a splash area for children, bike/walking/running trails, playgrounds, as well as places to sit and congregate (see Figure 3885: Illustration of Multi-Use Trail).



*Figure 3885: Illustration of Multi-Use Trail*

**Sec. 24-3886.** *Virginia Center Commons (VCC) Area Subdistrict: Regulating and Street Hierarchy Plan*

The Regulating and Street Hierarchy Plan for the Virginia Center Commons Area Subdistrict is set out in Figure 3886: Virginia Center Commons Area Regulating and Street Hierarchy Plan.

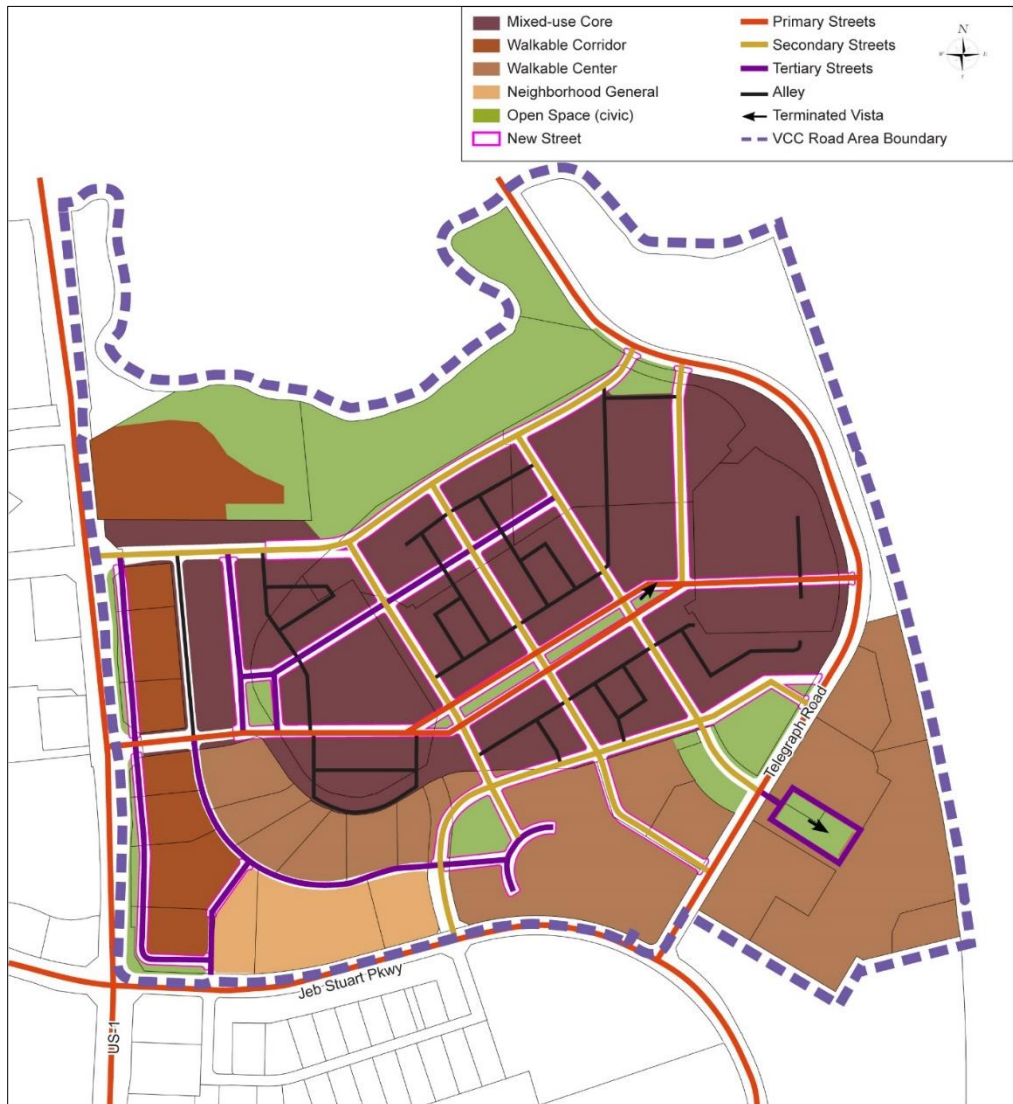


Figure 3886: Virginia Center Commons Area Regulating and Street Hierarchy Plan